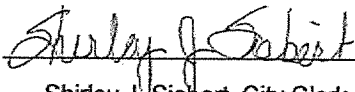


STATE OF ILLINOIS            )  
  ) SS  
COUNTY OF DU PAGE        )

I, Shirley J. Siebert, City Clerk of Wood Dale, Illinois DO HEREBY CERTIFY that as such City Clerk and keeper of the records, that the foregoing is a true and correct copy of Ordinance **#O-19-004 AN ORDINANCE APPROVING A SPECIAL USE, A PLANNED UNIT DEVELOPMENT AND MAJOR SITE PLAN REVIEW FOR OAKWOOD COMMERCE CENTRE, 700-770 N. WOOD DALE ROAD, IN THE CITY OF WOOD DALE** Passed to The City Of Wood Dale, Dupage County, Illinois, IN WITNESS WHEREOF, I have hereunto Subscribed my name and affixed the seal of the City of Wood Dale, this 21ST day of March, 2019.

  
Shirley J. Siebert, City Clerk

SEAL



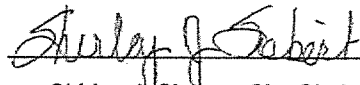
#O-19-004

**AN ORDINANCE APPROVING A SPECIAL USE, A PLANNED UNIT DEVELOPMENT  
AND MAJOR SITE PLAN REVIEW FOR OAKWOOD COMMERCE CENTRE, 700-770 N.  
WOOD DALE ROAD, IN THE CITY OF WOOD DALE**

Passed: March 21, 2019  
Published in Pamphlet Form March 21, 2019

I, Shirley J. Siebert, as the Clerk for the City of Wood Dale, hereby certify that the attached Ordinance is a true and correct copy of #O-19-004  
**AN ORDINANCE APPROVING A SPECIAL USE, A PLANNED UNIT DEVELOPMENT  
AND MAJOR SITE PLAN REVIEW FOR OAKWOOD COMMERCE CENTRE, 700-770 N.  
WOOD DALE ROAD, IN THE CITY OF WOOD DALE**

Passed and approved by the City Council of the City of Wood Dale March 21, 2019,  
hereby published in pamphlet form March 21, 2019.

  
Shirley J. Siebert, City Clerk

SEAL



ORDINANCE NO. O-19-004

**AN ORDINANCE APPROVING A SPECIAL USE, A PLANNED UNIT DEVELOPMENT  
AND MAJOR SITE PLAN REVIEW FOR OAKWOOD COMMERCE CENTRE,  
700-770 N. WOOD DALE ROAD, IN THE CITY OF WOOD DALE**

**WHEREAS**, the City of Wood Dale (the "City") is a body politic and corporate, organized and existing pursuant to the Illinois Municipal Code, 65 ILCS 5/1-1-1 *et seq*; and

**WHEREAS**, Bridge Industrial Acquisition LLC as contract purchaser("Petitioner") with the approval of the current owner, has petitioned the City for approval of a Special Use, a Planned Unit Development, Final Plat of Subdivision and a Major Site Plan Review for the improvement of the Property being legally described as follows:

**PARCEL 1:**

LOTS 3 AND 4 IN OAKWOOD COMMERCE CENTRE RESUBDIVISION OF LOTS 3, 4, 6 AND 7, BEING A RESUBDIVISION OF LOTS 3, 4, 6 AND 7 OF OAKWOOD COMMERCE CENTRE SUBDIVISION, BEING A RESUBDIVISION OF LOTS 201 THROUGH 213 IN FOREST CREEK UNIT #2, BEING A SUBDIVISION OF PART OF THE SOUTH HALF OF SECTION 4, TOGETHER WITH THE NORTH HALF OF SECTION 9, TOWNSHIP 40 NORTH, RANGE 11, EAST OF THE THIRD PRINCIPAL MERIDIAN, ACCORDING TO THE PLAT OF SAID OAKWOOD COMMERCE CENTRE RESUBDIVISION OF LOTS 3, 4, 6 AND 7 RECORDED APRIL 9, 1987 AS DOCUMENT NO. R87-49243, IN DUPAGE COUNTY, ILLINOIS.

**PARCEL 2:**

LOT 5 IN OAKWOOD COMMERCE CENTRE, BEING A RESUBDIVISION OF LOTS 201 THROUGH 213, BOTH INCLUSIVE, IN FOREST CREEK UNIT 2, BEING A SUBDIVISION OF PART OF THE SOUTH HALF OF SECTION 4, TOGETHER WITH THAT PART OF THE NORTH HALF OF SECTION 9, TOWNSHIP 40 NORTH, RANGE 11, EAST OF THE THIRD PRINCIPAL MERIDIAN, ACCORDING TO THE PLAT OF SAID OAKWOOD COMMERCE CENTRE RECORDED MARCH 15, 1985 AS DOCUMENT R85-18184, IN DUPAGE COUNTY, ILLINOIS.

**PARCEL 3:**

LOT 1 IN OAKWOOD CONSOLIDATION OF LOTS 1 AND 2, BEING A RESUBDIVISION OF LOTS 1 AND 2 IN OAKWOOD COMMERCE CENTRE, BEING A SUBDIVISION OF THE NORTH HALF OF SECTION 9, ALL IN TOWNSHIP 40 NORTH, RANGE 11 EAST OF THE THIRD PRINCIPAL MERIDIAN, ACCORDING TO THE PLAT OF SAID OAKWOOD CONSOLIDATION OF LOTS 1 AND 2 RECORDED DECEMBER 12, 1986 AS DOCUMENT NO. R86-157907, IN DUPAGE COUNTY, ILLINOIS.

Commonly known as 700-770 N. Wood Dale Road, PINs 03-09-205-020, -025, -026, and -027 (the "Property"); and

**WHEREAS**, a Special Use to allow for a Planned Unit Development is authorized under sections 17.403, 17.405, 17.503 of the Unified Development Ordinance ("UDO") in the I-1, Light Industrial District and the Wood Dale Road Frontage Sub-Area of the Thorndale Corridor Corporate District; and

**WHEREAS**, on February 25, 2019, the Community Development Commission held a public hearing on the proposed Special Use, Planned Unit Development, Final Plat of Subdivision and Major Site Plan Review, following the necessary publication of a Legal Notice pursuant thereto, as required by law; and

**WHEREAS**, the Community Development Commission considered the facts and testimony for Case Number 2018-CDC-09 and determined that the proposed Oakwood Commerce Centre Development Plan meets the standards of approval for a Special Use, Planned Unit Development, Final Plat of Subdivision and Major Site Plan Review and that the Oakwood Commerce Centre Development Plan is consistent with the UDO and Comprehensive Plan; and in making its determination, the Community Development Commission adopted the findings of fact included within the Staff Memorandum, dated February 25, 2019, attached hereto and incorporated by reference as Exhibit A, as the findings of the Community Development Commission and recommended to the City Council approval of Case No. 2018-CDC-09 with the following conditions:

- (1) The Special Use, Planned Unit Development, Final Plat of Subdivision and Major Site Plan Review shall substantially conform to the staff memo dated February 25, 2019 and the attached exhibits, except as such plans may be modified to meet City code requirements. Where deviations to the codes have not been requested as part of this approval, the existing codes must be met, as applicable, at the time of permit application.
- (2) Prior to the issuance of any permit (demolition, site (development) or building) for the subject site, the following must be completed:
  - a. Demonstrate compliance with the minimum fire flows for each proposed building.
  - b. Determine the final connection detail for the sidewalk and crosswalk on Wood Dale Road at Mittel Drive subject to approval by the City Engineer. The preferred connection would be to avoid the jog to the south.
  - c. Determine the impact of providing the queue lengths specified in the IDOT Design Guidelines to create dedicated right-turn and left-turn lanes on Mittel Drive. If the work would be cost prohibitive but provides a greater benefit to the neighborhood, the City Council may consider a cost-sharing agreement and revision



to the plan without a revision to the PUD. This condition does not require details of any potential revision to be completed before permits can be issued. The final direction for modifications to Mittel Drive must be agreed upon prior to issuance of a Certificate of Occupancy.

- (3) Prior to issuance of any Certificate of Occupancy:
  - a. The multi-use trail must be constructed and accepted by the City and DuPage County for the length of the subject property along Wood Dale Road.
  - b. A Public Utility Easement, subject to review and approval by the City Engineer, shall be recorded over the water main and storm sewers.
  - c. A Shared Access Easement, subject to review and approval by the Development Administrator, shall be recorded over the drive aisle and parking lot between Buildings 1 and 2.
  - d. A Stormwater Management Easement, subject to review and approval by the City Engineer and City Attorney, shall be recorded over portions of the regional detention ponds that are located on the subject property.
  - e. The draft Declaration of Easements, Covenants, Conditions and Restrictions shall be finalized and, subject to approval of the City Attorney, shall be recorded after conditions 3.a, 3.b and 3.c have been met.
  
- (4) Extend the multi-use trail to School Street within five (5) years of approval of the PUD. The City acknowledges that this work may be subject to approval of adjacent property owners, as a result of which additional time has been allotted for completion of the trail. Upon issuance of the site development permit for the subject site, a bond shall be provided and maintained in the amount of 110% of the cost of constructing the multi-use trail to School Street (only the portion that extends beyond the subject site) until such time as the trail extends to School Street. In the event the trail does not extend to School Street by the end of the five (5) years after the approval of the PUD, and the parties have not entered into an agreement to extend the time for installation of the trail, the City shall be entitled to draw upon the bond in order to complete the required improvements.

**WHEREAS**, the Planning, Zoning and Building Committee of the City Council of the City of Wood Dale reviewed the findings made by the Community Development Commission, and recommended that the Corporate Authorities approve the request; and

**WHEREAS**, the Corporate Authorities of the City of Wood Dale have received the recommendation of the Planning, Zoning and Building Committee and of the Community Development Commission, and have reviewed the findings made relative to the application, and upon its consideration, determined to approve the request for Special Use, Planned Unit Development, Final Plat of Subdivision and Major Site Plan Review.

**NOW, THEREFORE, BE IT ORDAINED BY THE CORPORATE AUTHORITIES OF THE CITY OF WOOD DALE, DUPAGE COUNTY, ILLINOIS**, as follows:

**SECTION ONE:** The recitals set forth hereinabove are incorporated herein by reference as part of this Ordinance.

**SECTION TWO:** The Property is hereby granted a Special Use, Planned Unit Development and Major Site Plan Review, pursuant to the staff memorandum dated February 25, 2019 (Exhibit A) for the improvement of the property identified as 700-770 N. Wood Dale Road subject to the following conditions:

- (1) The Special Use, Planned Unit Development, Final Plat of Subdivision and Major Site Plan Review shall substantially conform to the staff memo dated February 25, 2019 (Exhibit A), except as such plans may be modified to meet City code requirements. Where deviations to the codes have not been requested as part of this approval, the existing codes must be met, as applicable, at the time of permit application.
- (2) Prior to the issuance of any permit (demolition, site (development) or building) for the subject site, the following must be completed:
  - a. Demonstrate compliance with the minimum fire flows for each proposed building.
  - b. Determine the final connection detail for the sidewalk and crosswalk on Wood Dale Road at Mittel Drive subject to approval by the City Engineer. The preferred connection would be to avoid the jog to the south.
  - c. Determine the impact of providing the queue lengths specified in the IDOT Design Guidelines to create dedicated right-turn and left-turn lanes on Mittel Drive. If the work would be cost prohibitive but provides a greater benefit to the neighborhood, the City Council may consider a cost-sharing agreement and revision to the plan without a revision to the PUD. This condition does not require details of any potential revision to be completed before permits can be issued. The final direction for modifications to Mittel Drive must be agreed upon prior to issuance of a Certificate of Occupancy.

- (3) Prior to issuance of any Certificate of Occupancy:
- a. The multi-use trail must be constructed and accepted by the City and DuPage County for the length of the subject property along Wood Dale Road.
  - b. A Public Utility Easement, subject to review and approval by the City Engineer, shall be recorded over the water main and storm sewers.
  - c. A Shared Access Easement, subject to review and approval by the Development Administrator, shall be recorded over the drive aisle and parking lot between Buildings 1 and 2.
  - d. A Stormwater Management Easement, subject to review and approval by the City Engineer and City Attorney, shall be recorded over portions of the regional detention ponds that are located on the subject property.
  - e. The draft Declaration of Easements, Covenants, Conditions and Restrictions shall be finalized and, subject to approval of the City Attorney, shall be recorded after conditions 3.a, 3.b and 3.c have been met.
- (4) Extend the multi-use trail to School Street within five (5) years of approval of the PUD. The City acknowledges that this work may be subject to approval of adjacent property owners, as a result of which additional time has been allotted for completion of the trail. Upon issuance of the site development permit for the subject site, a bond shall be provided and maintained in the amount of 110% of the cost of constructing the multi-use trail to School Street (only the portion that extends beyond the subject site) until such time as the trail extends to School Street. In the event the trail does not extend to School Street by the end of the five (5) years after the approval of the PUD, and the parties have not entered into an agreement to extend the time for installation of the trail, the City shall be entitled to draw upon the bond in order to complete the required improvements.

**SECTION THREE:** That all Ordinances and Resolutions, or parts thereof, in conflict with the provisions of this Ordinance are, to the extent of such conflict, expressly repealed.

**SECTION FOUR:** That the City Clerk of the City of Wood Dale be and is directed hereby to publish this Ordinance in pamphlet form, pursuant to the statutes of the State of Illinois.

**SECTION FIVE:** That this Ordinance shall be in full force and effect from and after its passage, approval, and publication in the manner provided by law.

PASSED this 21st day of March, 2019.

AYES: ALDERMEN CATALANO, JAKAB, MESSINA, SORRENTINO,  
SUSSMARRSKI, E. WESLEY, AND WOODS  
NAYS: NONE

ABSENT: ALDERMAN R. WESLEY

APPROVED this 21st day of March, 2019.

SIGNED: Annunziato Pulice  
Annunziato Pulice, Mayor

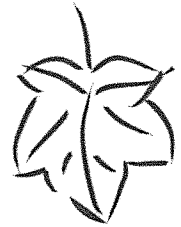
ATTEST: Shirley J. Siebert  
Shirley J. Siebert, City Clerk

Published in pamphlet form March 22, 2019

EXHIBIT A  
STAFF MEMORANDUM DATED FEBRUARY 25, 2019

# CITY OF WOOD DALE

Community Development



## MEMO

DATE: February 25, 2019

TO: Community Development Commission

FROM: Kelley Chrisee, AICP, Assistant Community Development Director

SUBJECT: Case No. 2018-CDC-09, Special Use, Planned Unit Development, Final Plat of Subdivision and Major Site Plan Review for Oakwood Commerce Centre, 700-770 N Wood Dale Road

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### REQUEST

An application has been submitted requesting a Special Use, Planned Unit Development, Final Plat of Subdivision and Major Site Plan Review to redevelop the Oakwood Commerce Centre located at 700-770 N Wood Dale Road (PINs 03-09-205-020, -025, -026, -027).

### PROPERTY INFORMATION

Address: 700-770 N Wood Dale Road  
PINs: 03-09-205-020, -025, -026, -027  
Property Size: 21.82 Acres (950,456 square feet)  
Existing Land Use: Office  
Future Land Use: Office and Warehouse  
Existing Zoning: I-1, Light Industrial – base zoning; Wood Dale Road Frontage Sub-Area of the Thorndale Corridor Corporate District

#### Surrounding Land Use & Zoning

North: I-1, Industrial and I-2, Industrial  
South: I-1, Industrial  
East: R-1, Estate Residential; R-4, Medium Density Single Family; and Unincorporated DuPage County R-3, Single-Family Residence District  
West: I-1, Industrial and I-2, Industrial

## ANALYSIS

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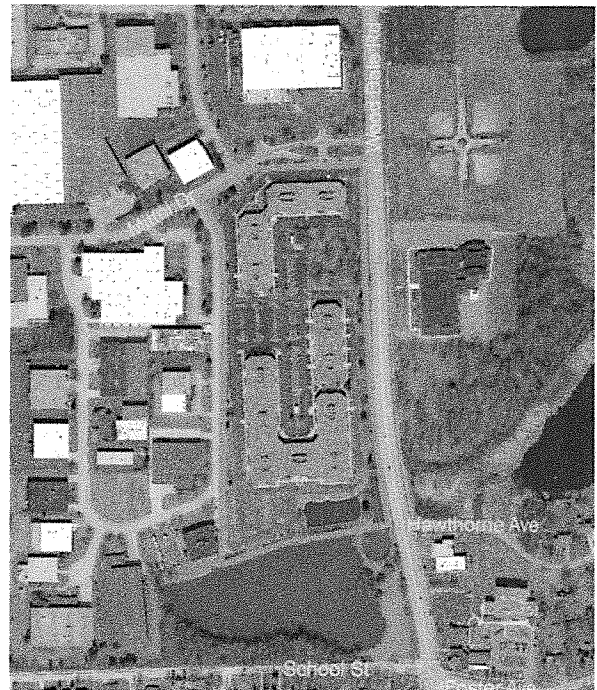
### Submittals

The analysis and recommendation provided within this memo are based on the following documents, which are on file in the Community Development Department and attached as noted:

- Community Development Commission & Annexation Application
- Topographic Survey (Exhibit A)
- Wetland Delineation Report
- Petitioner Narrative & Responses to Standards (Exhibit B)
- Final Plat of Subdivision (Exhibit C)
- Preliminary Construction Schedule
- Architectural Site Plans & Elevations (Exhibit D)
- Preliminary Improvements for Oakwood Commerce Center Redevelopment Drawings (Exhibit E)
- Traffic Impact Study
- Circulation Drawings – Autoturn Exhibits & Circulation Exhibit (Exhibit F)
- Tree Preservation & Landscape Plans (Exhibit G – only sheets L4, L5 & L6)
- Photometric Plan
- Sanitary Sewer Flow Analysis
- Kane-DuPage Soil and Water Conservation District Land Use Opinion 18-117
- Consultation for Endangered Species Protection and Natural Areas Preservation
- Determination letter from the Illinois Department of Natural Resources
- Declaration of Easements, Covenants, Conditions and Restrictions (Exhibit H)

### Project Description

The subject property, commonly known as Oakwood Commerce Center, is located at the southwest corner of Wood Dale Road and Mittel Drive (see map to the right). The zoning of the subject property is Wood Dale Road Frontage sub-area of the Thorndale Corridor Corporate Overlay with I-1, Light Industrial, underlying zoning. The site consists of nearly 22 acres and is improved with three single-story office buildings, surface parking and surface detention (see Exhibit A). The applicant has petitioned for Special Use, Planned Unit Development (PUD), Final Plat of Subdivision and Major Site Plan Review approval (see Exhibit B) to demolish the existing structures and build two modern corporate office and warehouse facilities.



*Location Map – 700-770 N Wood Dale Road*

The property will be re-subdivided to consolidate four lots into two – one for each building as depicted in Exhibit C. Lot 1 will contain an approximately 242,000 square foot building with 175 parking spaces and a truck court. Lot 2 will consist of an approximately 100,000 square foot building with 183 parking spaces, a truck court and surface detention. The architectural site plans and building elevations are provided as Exhibit D.

The prospective tenant of Building 1 is a current Wood Dale logistics business that occupies two buildings and would be looking to consolidate operations and relocate their corporate headquarters as well. The prospective tenant would use the majority of the building for warehousing with limited truck traffic. Unfortunately, the petitioner has not received confirmation that the prospective tenant has selected this site, as they are still considering options. Without confirmation from the prospective tenant, Building 1 is being proposed as quasi-speculative but tailored to the prospective tenant's needs.

The tenant for Building 2 is Forward Space, a Chicago-based furniture distributor that is consolidating from three other locations and will bring approximately 100 jobs to the community. This building will house their corporate headquarters, a showroom and warehouse space as an accessory use. The City has executed a Letter of Intent to share sales tax revenue, which will be formalized in an Economic Incentive Agreement upon approval of the development.

The construction is proposed to occur for both buildings simultaneously with substantial completion of the building shells and site anticipated in January 2020. Despite Building 1 being proposed as a quasi-spec building, its construction will occur at the same time as Building 2 in order to minimize major construction after Forward Space has taken occupancy. Forward Space anticipates occupying the building for the summer of 2020.

#### **Compliance with the Comprehensive Plan**

The subject property is designated as Office in the Future Land Use Map of the Comprehensive Plan. This property is also designated on the Future Land Use Map as a site likely to experience development pressure due to the weak office market. As such, the Comprehensive Plan identifies that this site could transition to an industrial use if the design were of high quality and if the traffic generated by the development could be handled with the capacity of existing or improved roadways.

The subject property is proposed to house a corporate office and industrial user in one building and a potential corporate headquarters for a logistics company in the other building. Both users would have an office component and a non-traditional warehouse use where there would be limited truck traffic. The proposed development located at the "front door" to the community, provides an ideal setting for the proposed complimentary and compatible land uses that can also utilize the existing road network and enhance the overall quality of life in Wood Dale. The proposed redevelopment will serve to attract a new business to Wood Dale and possibly allow an existing business to expand within the community.

As such, the proposed development would support the following Goals and Objectives (with a brief description of how they are being met):



- Goal 1: Connect Wood Dale Citizens to...
  - Objective 2: Increase walking and biking amenities (constructing sidewalks and multi-use path)
  - Objective 3: Increase opportunities for Wood Dale residents to work in Wood Dale (at least one new business is relocating to Wood Dale with approximately 100 jobs)
- Goal 2: Build Community Capacity
  - Objective 1: Seek opportunities for economic development (applying Special Use and PUD to support the redevelopment of an obsolete office park)
  - Objective 2: Expand job opportunities in the community (Forward Space employs corporate executives, interior designers and installers among others)
- Goal 3: Embrace Small-Town Charm
  - Objective 2: Enhance the appearance and “curb appeal” of commercial corridors and residential areas (high quality building design and landscaping)
- Goal 4: Keep Wood Dale Diverse
  - Objective 2: Manage development to create a balanced mix of land uses, promoting economic vitality and a sustainable quality of life (remaining current on the changing real estate market’s needs for contemporary industrial uses that can be located in the city and complement existing industries)
- Goal 5: Protect Land Values
  - Objective 3: Encourage investment through redevelopment and by attracting new visitors and residents (balance land use decisions to maintain a strong tax base and minimize property tax burden on residents)

The Special Use, Planned Unit Development, Final Plat of Subdivision and Major Site Plan Review requests are consistent with the Future Land Use and the Comprehensive Plan.

### **Compliance with the Unified Development Ordinance (UDO)**

The subject property is located within the Wood Dale Road Frontage sub-area of the Thorndale Corridor (TCC) Overlay District with I-1, Light Industrial underlying zoning. The subject property is not located within a floodplain but wetlands are present adjacent to the regional detention ponds serving the area. The property, as currently configured, contains four lots and is proposed to be re-subdivided into two lots – one for each building proposed (Exhibit C). Having street frontage on three sides, this property is considered a through, corner lot.

#### *Subdivision*

The subject property will be subdivided into two lots – one for each proposed building. Lot 1 will contain 13 acres and Lot 2 will contain 8.5 acres as depicted in Exhibit C. Both lots will exceed the minimum lot area of two acres.

As part of the subdivision, the petitioner is dedicating additional right-of-way (ROW) along East Pond Avenue and a portion of Mittel Drive. The additional ROW consists of 0.33 acres, which is currently a Utility Easement containing underground utilities. This dedication increases the total width of the ROW from 50-feet to 60-feet, which is closer to meeting the

minimum 66-foot ROW width specified for Type 4 thoroughfares (industrial streets). This thoroughfare type also requires 5-foot sidewalks to be installed, which is proposed to be constructed along East Pond Avenue and Mittel Drive as part of the redevelopment. The proposed sidewalk along Mittel Drive will connect to the existing crosswalk on Wood Dale Road. However, the final details will need to be reviewed and approved by the City Engineer in an attempt to avoid jogging the sidewalk to the south at the intersection.

Wood Dale Road is an arterial street that requires a ROW width of 100-feet according to the Type 3 (arterial) thoroughfare regulations. Since the ROW is already 100-feet wide, no additional ROW dedication is needed but a 12-foot wide trail is required. DuPage County was consulted regarding this regulation and has determined that, although there is sufficient parkway to provide a trail, there are many conflicts with existing power poles. As such, the redevelopment will provide a multi-use trail (bike path) along Wood Dale Road but mostly on private property and within the Utility and Greenway Easement. The use of the easement for a trail is consistent with the intent of the easement as stated in the original Forest Creek subdivision documents. The goal of the trail is to connect the on-street bike lane on Mittel Drive south to School Street and, eventually, to Salt Creek Greenway Trail (see map to the right). In order to facilitate the connection to School Street, the trail will likely need to be constructed on a portion of the property containing the regional detention basin (common area), as the remaining stretch of ROW to School Street also has obstructions. Thus, this connection will require coordination and approval by the Forest Creek Owner's Association and/or the owner of the common area. The petitioner has agreed to construct the trail to School Street provided that approval can be granted and final details determined.

DuPage County is supportive of the construction of the trail as it will support recommendations from the Elgin O'Hare Regional Bicycle and Pedestrian Plan completed in Spring 2017. However, they recommend a standard 10-foot wide trail. As such, a deviation is requested to reduce the trail width from 12-feet required by Sec. 17.703.D.2.c of the Municipal Code to 10-feet.



*DuPage County Trails Map with Proposed Trail Connection*

DuPage County is supportive of the construction of the trail as it will support recommendations from the Elgin O'Hare Regional Bicycle and Pedestrian Plan completed in Spring 2017. However, they recommend a standard 10-foot wide trail. As such, a deviation is requested to reduce the trail width from 12-feet required by Sec. 17.703.D.2.c of the Municipal Code to 10-feet.

Street trees are required according to Sec. 17.703.D.2.d of the Municipal Code. Where there is sufficient parkway and no conflicts with underground utilities exist, street trees will be provided. Per Sec. 6.503.D of the Municipal Code, planting of trees is prohibited if it will interfere with existing utilities. As such, street trees will not be planted along East Pond Avenue or within the narrow ROW on Mittel Drive (northwest corner of the property) where conflicts with existing utilities exist. Per DuPage County, street trees will not be permitted in the Wood Dale Road parkway due to potential conflicts with overhead wires and other utilities in the ROW. Since Wood Dale Road is not under City jurisdiction, the requirement for street trees does not apply to Wood Dale Road.

As required by code, street lights already exist at the intersections so no additional street lighting is proposed or required.

*Allowable Uses*

Forward Space will occupy Building 2 where the principal use would be Professional Office with accessory uses of Product Showroom and Warehouse. Professional Office, as the principal use, is permitted in the Wood Dale Road Frontage sub-area of the Thorndale Corridor Corporate (TCC) District. The tenant of Building #1 is not yet determined but if it were the existing Wood Dale logistics business, the use could possibly be Professional Office with Warehouse and Distribution as an accessory use. However, the petitioner has requested a PUD with a deviation to allow the Warehouse and Distribution use as a principal use for Building 1 in the Wood Dale Road Frontage sub-area of the TCC in the event that the proposed user does not commit. Granting this deviation would be consistent with the changing market conditions that have prevented occupancy of this site as office, which has remained largely vacant for more than eight years.

A PUD is allowed as a Special Use in the I-1, Light Industrial, Zoning District per the Table of Permitted Uses in Sec. 17.503.P of the Municipal Code. As such, the standards for Special Use, PUD and Major Site Plan Review have been evaluated and are provided for consideration beginning on page 11.

*Lot Development Standards*

Being in the Wood Dale Road Frontage sub-area of the TCC, the Block J Development Framework applies. The following table summarizes the regulations and how the redevelopment meets code requirements. See Exhibits D and E for detailed plans. Note that regulations **bold** indicate where deviations are being requested.

Regulation Type	Required/Allowed	Lot 1	Lot 2
a. Building Height	140-feet max.	38-feet	38-feet
b. Build to Line	50-feet along Wood Dale Rd and Mittel Dr (70% to be held by buildings)	75-feet – building aligns with platted building line <sup>1</sup>	<b>82.3-111.3-feet</b> – building aligns with adjacent building <sup>2</sup>

Regulation Type	Required/Allowed	Lot 1	Lot 2
c. Parking Setback	100-feet min.	Center parking lot – <b>approx. 95-feet (Wood Dale)</b> ; approx. 145-feet (Pond)	Center parking lot – <b>approx. 95-feet (Wood Dale)</b> ; approx. 145-feet (Pond)
		North parking lot – <b>approx. 75-feet (Wood Dale)</b> ; <b>50.5-feet (Mittel)</b> ; approx. 210-ft (Pond)	South parking lot – approx. 155-feet (Wood Dale); approx. 180-feet (Pond)
		Proposed future stalls – 121.5-feet (Pond); approx. 270-feet (Mittel)	Proposed future stalls – approx. 140-feet (Pond)
d. Landscape Buffer			
Wood Dale Rd	50-feet min.	50.5-feet	55-90-feet
Mittel Dr	50-feet min.	n/a	Approx. 65-feet
Pond Dr	50-feet min.	65-85-feet	85-feet
South property line	50-feet min.	n/a	<b>48-feet<sup>3</sup></b>

<sup>1</sup> Sec. 17.601.Q states, "If a recorded subdivision plat imposes a building or setback line for a lot which is different than what is required by the applicable section of this article, then whichever is greater shall apply." A deviation is not required for the build-to line for Lot 1.

<sup>2</sup> The building line for Lot 2 was originally platted to not follow the curve of the road. When the subject property was re-subdivided in 1983 the platted building line did follow the road but it is unclear if that was the original intention. Given the existing regional detention pond located on the subject property, Lot 2 will require a deviation to allow the building to be setback 82.3-feet at the northern most corner and 111.3-feet at the southernmost corner.

<sup>3</sup> A deviation has been requested to allow the patio to encroach 2-feet into the 50-foot landscape buffer along the south property line. This deviation is required to accommodate the required 26-foot fire lane in the shared parking lot.

Where the development framework in the TCC is silent, then the I-1, Light Industrial, and Type 14 Building regulations apply. The following table summarizes the applicable regulations of Sec. 17.605.C.14 of the Municipal Code. Regulations in **bold** indicate where deviations are being requested.

Regulation Type	Required/Allowed	Lot 1	Lot 2
a. Development District	I-1, I-2	I-1 underlying zoning (applies where TCC regulations are silent)	
<b>b. Lot Standards (Lot 1):</b>			
Lot Width	200 ft	Approx. 580 ft	Approx. 650 ft
Lot Depth	140 ft	Approx. 815 ft	Approx. 610 ft
Lot Area	2 acres	13.00 acres (566,141 sq ft)	8.49 acres (369,989 sq ft)
Lot Coverage (max.)	80%	73.9% (418,568 sq ft)	59.9% (221,523 sq ft)

Regulation Type	Required/Allowed	Lot 1	Lot 2
<b>c. Building Placement Standards</b>			
Permitted yard obstructions	See 17.602.A.2.b		
<b>d. Frontage Types</b>	Front lawn; Shopfront	n/a	n/a
<b>f. Building Use Standards</b>			
Ground Floor	Office/ Manufacturing/ Storage	Office/ Storage	Office/ Storage
Upper Floors	Office/Storage	Office	n/a
<b>g. Parking Standards</b>			
Parking spaces	Lot 1 - 363 [(241,888/1,000)*1.5] Lot 2 - 151 [(100,378/1,000)*1.5] <sup>1</sup>	<b>175 with potential for 106 future</b> (see table in Parking and Traffic for more details)	183 with potential for 36 future (see table in Parking and Traffic section for more details)
	Both Lots Combined - 514	<b>358 (500 with 142 future)</b>	
Accessible spaces	Lot 1 – 6 (175 total) Lot 2 – 6 (183 total)	6	6
Loading Berths	Lot 1 – 4 Lot 2 – 3	39 exterior docks; 2 drive-in doors	12 exterior docks; 2 drive-in doors

<sup>1</sup> The principal use of this property is professional office but the more appropriate parking ratio is the Business Park. A deviation is requested to utilize the Business Park parking ratio due to the distribution of office and storage space within the building.

### Parking and Traffic

Access to the redeveloped site will generally remain the same with one full access on Wood Dale Road, full access on Mittel Drive and two access points on East Pond Avenue. The access on Wood Dale Road is shifting south to align with the drive aisle between the two buildings. The existing center median will be removed but a dedicated right-turn and left-turn lane will be provided. The maximum driveway width (curb cut) at the property line is 34-feet and 44-feet at the street per Sec. 17.607.D.3.c(3)(C) of the Municipal Code. In order to provide the dedicated turn lanes, a deviation is needed to allow a driveway width of 39.9-feet at the property line. Since this driveway is accessed from Wood Dale Road, the County will approve the width of the curb cut at the street. The existing access on Mittel Drive will be replaced to provide a crosswalk. While the widths of the access points are not changing, the width for ingress and egress are at 17-feet in width, which meets the code requirements at the property line. However, the width at the street requires a deviation to allow the existing width of 93.8-feet to remain. The northern access point on East Pond Avenue will be modified to allow for tractor trailer traffic, requiring a 51.4-foot width at the property line and 109.5-feet at the street. The southern access point on East Pond Avenue will be shifted and widened to allow for tractor trailer traffic, requiring a 48.5-foot width at the property line and 104.6-feet at the street. These deviations are required to accommodate the tractor trailer and emergency vehicle traffic as depicted in the Autoturn Diagrams in Exhibit F. Moreover, the narrow ROW width and shallow parkway depth exacerbate the issue. Under a PUD, this type of variation from the code is called a deviation but other variances have been granted in similar circumstances.

The proposed development will be served by three parking areas. Lot 1 will have dedicated parking north of the building along Mittel Drive and Lot 2 will have dedicated parking south of the building. The parking lot between the buildings (shared parking lot) can provide parking for either building, as this area will be included in the cross-access easement to be recorded upon completion of the improvements. The details of the parking distribution are identified in the table below. Requests for deviations are indicated in **bold** text.

	Dedicated (on the lot)	Shared (used by either lot)	Total Available	Future (can be accommodated)
Lot 1	138	37	<b>175</b>	106
Lot 2	146	37	183	36
<b>Total</b>	284	74	358 (500 with future)	142

Forward Space has indicated that they will have approximately 100 employees at this location but is continuing to grow the business. As such, they have indicated that they need 146 parking spaces. Based on the parking stalls that they need, there are overparked leaving the shared parking to Lot 1 for use.

The potential user for Lot 1 indicated that 175 parking stalls would meet their need. However, based on code requirements, the petitioner has requested a deviation from providing 363 parking spaces required on Lot 1. While the number of parking spaces located on Lot 1 total 175, there is the ability to use the other 37 parking spaces within the shared parking lot between the buildings that are excess. Additionally, if parking becomes an issue on Lot 1 and not all the loading docks are needed, up to 106 parking spaces can be provided. The petitioner has determined that based on similar uses on other properties, the number of parking stalls that the user would likely require is not more than 212.

In total, the number of required parking for the PUD is 514 and the maximum number of parking spaces that can be accommodated on the subject property is 500. That results in an overall shortage of 14 parking stalls, which is minor given the scale of the project.

The Circulation Diagram in Exhibit F depicts the areas predominately used by different types of traffic (pedestrian, passenger vehicles and commercial vehicles). Commercial vehicle traffic will exclusively use East Pond Avenue with general circulation and truck docks behind the buildings. Passenger vehicles will primarily use the Mittel Drive or Wood Dale Road access points to access one of the three parking lots. Pedestrian traffic can easily traverse the site with multiple sidewalks and a much needed connection to the trail system will improve pedestrian access for the neighborhood. The connection detail for how the sidewalk on Mittel connects to the existing crosswalk on Wood Dale Road has not yet been approved by the City Engineer, as the preferred connection is to avoid a jog to the south.

A Traffic Impact Study (TIS) was conducted to evaluate the potential impact on adjacent roadways. One of the major findings was that the potential traffic generated by the

redeveloped site is estimated to be less of an impact than if the existing office buildings were fully occupied. Other findings include:

- The proposed development will have little impact on existing roadway conditions.
- There is sufficient capacity of the adjacent roadways to accommodate the proposed redevelopment.
- Restricting truck traffic to East Pond Avenue entrances and passenger vehicles to Wood Dale Road and Mittel Drive entrances will adequately accommodate site traffic.

Based on existing conditions, recommendations to improve access and circulation are being incorporated into the redevelopment which include:

- Widening Mittel Drive at Wood Dale Road to provide full width dedicated turn lanes. The design currently requires an exception from IDOT's Design Guidelines for the required storage and taper lengths. This issue requires further discussion with the City Engineer to determine the impact of providing the required storage and taper lengths. If the proper guidelines are followed and there is a greater benefit to the neighborhood, the City should consider a cost-share to ensure the appropriate improvements are made.
- Signal adjustments to provide more green time to Mittel Drive, add a right-turn overlap phase and provide pedestrian signals and push buttons to accommodate the new crosswalk.
- The northbound left-turn lane on Wood Dale Road will be restriped to accommodate the shifted access to the subject property. The proposed restriping will not conflict with the southbound left-turn lane at Hawthorne.

The City Engineer has reviewed and recommends acceptance and approval of the recommendations in the TIS.

### *Landscaping*

The petitioner has prepared a draft Tree Preservation Plan with a tree inventory. There are a number of trees that will be removed in what is being dedicated as ROW along East Pond Avenue and Mittel Drive. As the removal of the trees will be in the newly dedicated ROW, the tree removal regulations do not apply. Additionally, the trees are being removed in order to accommodate the sidewalk required by code. There are a number of trees being removed in order to accommodate the multi-use trail on private property, due to multiple obstructions in the Wood Dale Road ROW. As such, a deviation is requested to not require replacements of the trees being removed to allow for the construction of the multi-use path.

A draft Landscape Plan has also been prepared that shows 245 replacement trees, which is depicted in Exhibit G. Despite the fact that these trees might "fit" on the site, the ability for all the vegetation to thrive is of concern. If the petitioner were to be required to plant so many trees, the design of the buildings would likely be covered by vegetation. As such, a deviation is requested to allow an administrative variance of up to 40% of the landscaping regulations to provide greater flexibility in landscaping while still keeping the site attractive, safe and comfortable. There will still be a significant number of replacement trees in

addition to the required landscaping to ensure the redeveloped site has a high quality of design.

#### *Lighting*

The only site lighting currently proposed is light fixtures mounted to the buildings. A site photometric plan was reviewed by the City Engineer who recommended approval subject to final details being provided at time of permit. Any revisions to the proposed lighting will be in accordance with Sec. 17.609 of the Municipal Code.

#### *Stormwater Management*

The subject site is currently served by a regional detention basin located south of the property with a portion of the connected ponds located on the southeast corner and southern edge of the subject site. The areas tributary to the regional detention basin total approximately 51 acres and the applicable properties receive an allocated storage volume. (Since one of the regional ponds extends onto the subject property, the petitioner shall grant a Stormwater Management Easement over the limits of the pond on the subject property. The easement provisions for this pond will not be consistent with the Drainage Easement provisions on the Final Plat of Subdivision for the onsite basin and must be reviewed and approved by the City Attorney prior to being recorded.) There is also a detention basin located entirely on the subject property that will be filled in and relocated to accommodate the redevelopment. The petitioner has factored in the allocation for the regional detention storage, the loss of the existing onsite detention basin and new impervious area to propose a wetland bottom detention basin in the southwest corner of the site. There will be no impact to the regional detention basin located on the subject property. The wetland-bottom detention basin will satisfy both stormwater detention and Post Construction Best Management Practices (PCBMPs) requirements for the development. The existing drainage pattern will be maintained where the site will drain to the onsite basin prior to being discharged to the regional detention basin. The City Engineer has reviewed and recommends approval of the proposed Stormwater Management design.

There are jurisdictional Waters of the US/wetlands associated with the regional detention ponds but the redevelopment will not impact those identified waters/wetlands. The proposed development must meet the buffer impact requirements in the Municipal Code. The City Engineer has recommended approval of the approach and will confirm the details upon review of the permit.

#### *Public Utilities*

The development will be served by a looped water main that will be tapped off the existing water main on East Pond Avenue and Mittel Drive. The petitioner is requesting a deviation from the spacing of fire hydrants from 300-feet required in Sec. 17.703.C.1.a(8) to 600-feet allowed in Sec. 507.5.1 of the 2012 International Fire Code (adopted in Sec. 12.301 of the Municipal Code). This spacing will be acceptable provided that the petitioner confirms that the minimum fire flows can be achieved. Stormwater runoff from both the onsite and off-site areas will be conveyed to the on-site/regional detention ponds using both new and existing storm sewers through the development. Two new sanitary services along the eastern



property line will service the proposed buildings and connect to the existing sanitary sewer on Wood Dale Road. No deviations are being requested in regard to the sanitary and storm sewer service for the development. Upon acceptance of the public utilities and prior to issuance of a Certificate of Occupancy, a grant of easement shall be recorded over the applicable utilities subject to review and approval by the City Engineer.

#### *Public Safety*

The Wood Dale Fire Protection District has reviewed the proposed redevelopment and determined that there is sufficient access for emergency vehicles in and around the site. A manual and automatic fire alarm system and an automatic suppression system will be installed throughout the proposed buildings. In order to verify the adequacy of fire flows, the petitioner has agreed to provide fire flow calculations prior to issuance of any permit. The Wood Dale Fire District has recommended approval of the proposed redevelopment subject to approval of the fire flow calculations.

#### *PUD and Subdivision Process*

The petitioner is requesting a combined concept, preliminary and final development plan/plat to facilitate construction commencement immediately upon approval. The requested Special Use, PUD, Final Plat of Subdivision and Major Site Plan Review is consistent with the UDO.

#### **Neighborhood Comment**

Notice was provided to adjacent property owners in accordance with Section 17.401.D of the UDO. Staff has not received any inquiries regarding the requests as of February 20, 2019.

#### **Findings of Fact**

The Community Development Commission may recommend approval of a Special Use, Planned Unit Development and Major Site Plan Review if evidence is presented to establish that the application meets the standards. The standards are as follows (*staff comments italicized*):

#### Special Use Standards

1. The proposed special use at the particular location requested is necessary or desirable to provide a service or a facility which is in the interest of the public and will contribute to the general welfare of the neighborhood or community;

*The single story office complex has been largely vacant for eight years. The market analysis conducted in conjunction with the Comprehensive Plan indicated that the subject site would likely experience development pressure due to the diminishing suburban office market and booming industrial market. The proposed redevelopment seeks to bring at least one new user housing a corporate headquarters with showroom and warehouse accessory uses to the site. The other potential user could be an existing Wood Dale business seeking to consolidate locations and relocate their corporate headquarters as well. Given that this property*

*is highly visible and effectively serves as the "front door" to the community, the design of the buildings has been enhanced to meet that status. One of the users will also generate sales tax revenue, in which the City Council has already executed a Letter of Intent to enter into an Economic Incentive Agreement. This standard is met.*

2. The proposed special use will not have a substantial adverse effect upon the adjacent property, the character of the neighborhood, traffic conditions, utility facilities and other matters affecting the public health, safety and general welfare;

*As identified in the staff memo, the proposed redevelopment would not have any greater traffic impact on the neighborhood than if the existing office buildings were fully occupied. In fact, the proposed redevelopment would have a substantially less impact on the neighborhood with respect to vehicular traffic and public utilities. Neither Forward Space or the potential user are heavy truck users but when trucks do enter the site, they will be restricted to East Pond Avenue. The redevelopment will greatly improve pedestrian mobility in and around the site with the construction of sidewalks and the multi-use trail along Wood Dale Road. The character of the neighborhood will be improved with two new, modern headquarters and industrial buildings that are occupied as opposed to an underutilized, obsolete property that exists today. This standard is met.*

3. The proposed special use will not be injurious to the use and enjoyment of other property in the immediate vicinity of the subject property for the purposes already permitted in such zoning district, nor substantially diminish and impair other property valuations within the neighborhood;

*The surrounding properties can continue to be used in the same manner as they are today or in accordance with the applicable zoning regulations. The redevelopment of the subject property should serve to promote investment in the adjacent properties to further increase property values in the neighborhood. This standard is met.*

4. The nature, location, and size of buildings or structures involved with the establishment of the special use will not impede, substantially hinder, or discourage the development and use of adjacent land and buildings in accord with the zoning district within which they lie;

*The two buildings proposed on the subject property will not impede, hinder or discourage the development of adjacent land and buildings. The design quality of the buildings should promote investment in the adjacent properties. This standard is met.*

5. The proposed special use will be designed, arranged and operated so as to permit the development and use of neighboring property in accordance with the applicable district regulations;

*As previously indicated, the use of the property is intended to be corporate headquarters and industrial. It is unlikely that the user of the larger building will be a heavy logistics user but the site design does allow for this type of user. The restriction of truck traffic to East Pond Avenue should minimize impacts to Wood Dale Road south of the site and will direct truck traffic to remain on industrial-type roadways. This standard is met.*

6. Adequate utilities, access roads, drainage, and/or other necessary facilities have been or will be provided;

*The City Engineer has determined that the proposed utilities, access points and drainage facilities are adequate and has recommended approval. This standard is met.*

7. Parking areas shall be of adequate size for the particular special use, which areas shall be properly located and suitably screened from adjoining residential uses, and the entrance and exit driveways to and from these parking areas shall be designed so as to prevent traffic hazards, eliminate nuisance, and minimize traffic congestion in the public streets; and

*There has been a request for a deviation from the number of required parking spaces for Lot 1 and from the 100-foot parking setback for the subject property. The parking spaces proposed meet the code requirements in terms of size, location and screening. The proposed parking areas and the resulting traffic will cause no greater traffic hazard, nuisance or traffic congestion as the fully occupied existing office buildings. This standard is met.*

8. Such other standards and criteria as are established by the ordinance for a particular special use as set forth in subsections D and H of this section, if applicable, and as applied to planned unit developments as set forth in Chapter 17, Article IV, Section 17.405.

*The standards for PUDs are evaluated below, per Section 17.405 of the UDO. This standard is met.*

#### General Standards and Criteria for Planned Unit Developments

1. The proposed development will not injure or damage the use, value and enjoyment of surrounding property nor hinder or prevent the development of surrounding property in accordance with the Wood Dale comprehensive plan.

*The Comprehensive Plan specifically identified this property as a site likely to experience development pressure. The proposed development is consistent with the type of development occurring on the west side of Wood Dale Road but with more attention paid to building and site design. The redevelopment of the subject property will not injure or damage the use, value and enjoyment of adjacent properties or discourage the development of surrounding properties. The design*

*quality of the proposed buildings should promote investment in the adjacent properties. This standard is met.*

2. The proposed development can be substantially completed within the period of time specified in the schedule of development submitted by the applicant.

*The proposed redevelopment will largely occur at the same time. It is anticipated that the site work and shell of both buildings will be substantially completed by January 2020. The interior build-out for Forward Space is estimated to be completed to allow the company to move in during the summer of 2020. Once the tenant has been confirmed for the other building, a construction schedule for the build-out will be developed. This standard is met.*

3. The entire tract or parcel of land to be occupied by the proposed development shall be held in a single ownership, or if there are two (2) or more owners, the application for such proposed development shall be filed jointly by all such owners.

*The subject property is owned by Wood Dale Road Investors, LLC. This standard is met.*

4. The development plan shall contain such proposed covenants, easements and other provisions relating to the bulk and location of buildings, uses and structures and public facilities as are necessary for the welfare of the planned unit development and are not inconsistent with the best interests of the city. Such covenants, easements and other provisions, when part of the approved final development, may be modified, removed or released only with the consent of the city council after a public hearing before, and recommendation by the community development commission as provided in this section.

*The proposed development will be governed by the draft Declaration of Easements, Covenants, Conditions and Restrictions (CCRs), included as Exhibit H. In addition to the CCRs, the petitioner will be required to grant easements for public utilities, cross access and stormwater management for the portion of the regional detention pond on the subject property. The recording of the CCRs will occur upon approval by the City Attorney and before any Certificates of Occupancy have been issued. This standard is met.*

5. Sanitary sewers, storm sewers and water supply to service the development are adequate to serve the proposed development and will not reduce existing capacity below that necessary to serve existing developments, or overload local facilities beyond design capacity.

*The Public Works Director and City Engineer have reviewed and recommend approval of the proposed design of the public utilities. This standard is met.*

6. The location and arrangement of structures, parking areas, walks, lighting and appurtenant facilities is compatible with the surrounding land uses, and any part of a

proposed development not used for structures, parking and loading areas, or access ways, is landscaped or otherwise improved.

*The proposed development exceeds the standards of adjacent properties, as the overlay regulations apply to the subject site but not to neighboring properties. Specifically, the parking areas are located behind the building line along Wood Dale Road, there is a 50-foot landscape buffer that exists on nearly all sides of the subject property and there is enhanced pedestrian connectivity that is lacking in the rest of the neighborhood. This standard is met.*

7. The project area is adaptable to unified development and shall have within or through the area no physical features that will tend to destroy the neighborhood or community cohesiveness. There is no minimum project area for a planned unit development.

*The proposed site design orients the buildings toward Wood Dale Road and Mittel Drive. The existing regional detention ponds will remain but will be enhanced as a site amenity with additional landscaping. The addition of sidewalks and the multi-use trail will help to create more cohesiveness within the neighborhood by providing increased pedestrian mobility. This standard is met.*

8. The uses permitted in the development are necessary or desirable and the need for such uses is clearly demonstrated by the applicant.

*As previously stated, the Comprehensive Plan identifies this site as one that will likely experience development pressure. The determination made in the Comprehensive Plan is a result of a market study that showed a declining suburban office market and a booming industrial market. The proposed uses actually combine the desire for corporate headquarters with a warehouse and distribution component. The proposed redevelopment already has one committed tenant and is continuing to work with the other tenant in the hopes of having the site fully leased prior to construction beginning. This standard is met.*

9. The dominant land use of the proposed planned unit development is consistent with the recommendations of the Wood Dale comprehensive plan for the area containing the project.

*The Wood Dale Comprehensive Plan identifies the need to accommodate industrial users and stated that this site could take advantage of the conversion to industrial if there were a high quality of design. The building and landscaping designs will not only improve the aesthetics of the site but help to welcome visitors to the community. This standard is met.*

10. Any modifications of the standards and specifications of this chapter or other regulations that would otherwise be applicable to the site are warranted by the design of the development plan, and the amenities incorporated in it, and are not inconsistent with the public general welfare.

*The subject property is located within the overlay district and, as such, compliance with design requirements is necessary. The proposed redevelopment is the first project requiring compliance with the overlay regulations. The combination of applying regulations for a site that was initially intended to be multi-story office requires some deviation to address existing market constraints. The deviations requested assist in providing a development plan that provides flexibility in users to respond to market conditions, enhanced pedestrian connectivity within and around the site and puts back into use a site that has been largely unoccupied for eight years. This standard is met.*

11. Exceptional landscaping features such as larger caliper, varied species and reduced spacing of trees and additional sodding above the minimum requirements specified in section 17.606 of Chapter 17 is provided.

*The proposed landscape plan features retaining as many existing trees as possible, maintaining a 50-foot landscaped buffer on nearly all sides and a variety of species. This standard is met.*

12. All proposed streets and driveways are adequate to serve the residents, occupants, visitors or other anticipated traffic of the planned unit development. Entrance points or locations of streets and driveways upon existing public roadways shall be subject to the approval of the city, and if applicable, the DuPage County highway department, and the Illinois department of transportation. If traffic control devices are required to prevent or relieve hazards or congestion on adjacent streets and the proposed control device is not within the normal or scheduled sequence of installations, the city council may require, as a condition of approval of a proposed planned unit development, such devices to be provided at the developer's cost.

*No new streets are being proposed for the redevelopment and, in fact, the existing driveways are largely remaining. Per the TIS, the redevelopment of the site has minimal impact on the existing roadway conditions. However, improvements recommended in the TIS will be implemented as part of the redevelopment. DuPage County has reviewed the proposed redevelopment and has no major concerns. This standard is met.*

13. Off street parking is conveniently accessible to all dwelling units and other uses in the planned unit development. Where appropriate, common driveways, parking areas, walks, and steps to parking and service areas are to be screened through ample use of trees, shrubs, hedges, landforms and walls.

*There are no dwelling units within the PUD and no off street parking is proposed. However, parking and service areas will be screened in accordance with the code requirements. This standard is met.*

14. A pedestrian circulation network is provided.

*Sidewalks are being constructed within the newly dedicated ROW along East Pond Avenue and Mittel Drive that connect in and around the subject site. The addition of a multi-use trail will eventually provide a currently missing connection from the on-street bike lane on Mittel Drive to the Salt Creek Greenway Trail. This standard is met.*

15. The planned unit development provides for underground installation of utilities (including electricity and telecommunications) in public ways and private extensions thereof. Provisions shall be made for acceptable design and construction of storm sewer facilities including grading, gutter, piping and treatment of turf to handle stormwater, prevent erosion and the formation of dust. Utilities and maintenance of facilities shall be in accordance with the requirements and regulations of the city as set forth in this chapter.

*The City Engineer has reviewed the plans and will confirm that all required utilities will be installed in accordance with applicable codes at the time of permit. This standard is met.*

16. The proposed planned unit development satisfies the applicable objectives as provided in subsection B of this section.

*The proposed redevelopment will support:*

- *Objective 3: functional and beneficial use of open space by providing better connectivity to existing open space and trail systems by constructing the multi-use trail along Wood Dale Road and providing 50-foot landscaped buffers along street frontages.*
- *Objective 5: Provision for a safe and desirable environment characterized by a sensitive and unified building and site development program by maintaining and creating a 50-foot landscaped buffer along street frontages, enhanced pedestrian connectivity within and around the site and high quality building design.*

*This standard is met.*

17. Existing ponds, creeks, rivers, lakes, wetlands or fens on or adjacent to the planned unit development are enhanced and protected from development.

*No modifications are planned to the existing regional detention ponds or wetlands. These are to be protected during construction to ensure that they are not negatively impacted. This standard is met.*

#### Standards For Site Plan Review:

1. The relationship of the site plan to the policies, goals and objectives of the comprehensive plan;

*As stated previously in the staff memo, the proposed redevelopment supports each goal of the Comprehensive Plan and many objectives. This standard is met.*

2. Traffic and parking layout so as to minimize danger and conflicts between pedestrians and motorists, and otherwise comply with the requirements of Chapter 17;

*As stated previously in the staff memo, the City Engineer has reviewed the TIS and recommends approval of the redevelopment. This standard is met.*

3. Location of principal structures, accessory structures and freestanding signs, so that the location of accessory structures and freestanding signs does not impede safe and efficient traffic circulation, stormwater drainage, or otherwise adversely impact adjoining land improvements;

*Per the City Engineer, safe and efficient traffic circulation and adequate stormwater drainage is being provided to minimize any adverse affects to adjacent properties. This standard is met.*

4. The development administrator may enlist the services of other city departments and consultants to determine compliance with the provisions of Chapter 17 and other provisions of the Municipal Code;

*The City Engineer has provided approval of the proposed site and stormwater management design as well as the approach to address potential traffic and parking impacts. The Fire District also concurs with the site plan with regards to access. This standard is met.*

5. That the proposed use(s) is/are permitted in the district in which the property is located;

*With the approved deviation to allow a Warehouse and Distribution use as a principal use for Lot 1, this standard is met.*

6. That the proposed arrangement of buildings, off street parking, access, lighting, landscaping, and drainage is compatible with adjacent land uses and employs sound site planning principles;

*The proposed redevelopment is compatible with adjacent land uses and employs sound planning principles. This standard is met.*

7. That the vehicular ingress and egress to and from the site and circulation within the site provides for safe, efficient and convenient movement of traffic not only within the site but on adjacent roadways as well; and

*The restriction of tractor trailer traffic to the entrances on East Pond Avenue and passenger vehicles using the entrances on Wood Dale Road and Mittel Drive provides for safe, efficient and convenient movement of traffic. The construction of sidewalks and the multi-use trail provides for the safe movement of pedestrian traffic as well. This standard is met.*

8. That all outdoor storage areas are screened and are in accordance with standards specified by this chapter.

*No outdoor storage is proposed. This standard is not applicable.*



Standards for Final Plat:

The Community Development Commission shall recommend approval and the City Council shall approve a Final Plat unless it makes written findings specifying the manner in which:

1. The design and layout of the subdivision does not conform to the provisions of the UDO.

*The proposed subdivision includes additional ROW dedication and all required improvements, including sidewalks and a multi-use trail. The design and layout of the subdivision does conform to the provisions of the UDO.*

2. The applicant has not made adequate provision to install improvements required by the Community Development Commission or City Council under authority of the UDO.

*The petitioner agrees to install all required public improvements as part of the redevelopment.*

3. The Final Plat fails to comply with an approved Preliminary Plat.

*The petitioner has requested a combined Concept, Preliminary and Final Plat approval process per Sec. 17.406.D of the Municipal Code. As such, this standard is not applicable.*

4. The Plat does not conform to the Comprehensive Plan, the Official Zoning Map, Article IV of Chapter 17 (UDO), City ordinances, or established planning policies of the City.

*The Final Plat of Subdivision conforms to the Comprehensive Plan, the Official Zoning Map, Article IV of Chapter 17, applicable City ordinances and planning policies of the City.*

**RECOMMENDATION**

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The Community Development Department finds that the request for a Special Use, Planned Unit Development, Final Plat of Subdivision and Major Site Plan Review to redevelop 700-770 N Wood Dale Road is compatible with surrounding zoning and land use classifications, meets the requirements in the Unified Development Ordinance and is consistent with the City's Comprehensive Plan. Based on the above considerations, staff recommends that the Community Development Commission make the following motion recommending approval of this petition:

Based on the submitted petition and the testimony presented, the proposed Special Use, Planned Unit Development, Final Plat of Subdivision and Major Site Plan Review meets the standards of approval and is consistent with the UDO and Comprehensive Plan; and, therefore, I move that the Community Development Commission adopt the findings of fact included within the staff memo as the findings of the Community Development Commission, and recommend to the City Council approval of Case No. 2018-CDC-09 subject to the following conditions:

1. The Special Use, Planned Unit Development, Final Plat of Subdivision and Major Site Plan Review shall substantially conform to the staff memo dated February 25, 2019 and the attached exhibits, except as such plans may be modified to meet City code requirements. Where deviations to the codes have not been requested as part of this approval, the existing codes must be met, as applicable, at the time of permit application.
2. Prior to issuance of any permit (demolition, site (development) or building) for the subject site, the following must be completed:
  - a. Demonstrate compliance with the minimum fire flows for each proposed building.
  - b. Determine the final connection detail for the sidewalk and crosswalk on Wood Dale Road at Mittel Drive subject to approval by the City Engineer. The preferred connection would be to avoid the jog to the south.
  - c. Determine the impact of providing the queue lengths specified in the IDOT Design Guidelines to create dedicated right-turn and left-turn lanes on Mittel Drive. If the work would be cost prohibitive but provides a greater benefit to the neighborhood, the City Council may consider a cost-sharing agreement and revision to the plan without a revision to the PUD. This condition does not require details of any potential revision to be completed before permits can be issued. The final direction for modifications to Mittel Drive must be agreed upon prior to issuance of a Certificate of Occupancy.
3. Prior to issuance of any Certificate of Occupancy:
  - a. The multi-use trail must be constructed and accepted by the City and DuPage County for the length of the subject property along Wood Dale Road.
  - b. A Public Utility Easement, subject to review and approval by the City Engineer, shall be recorded over the water main and storm sewers.
  - c. A Shared Access easement, subject to review and approval by the Development Administrator, shall be recorded over the drive aisle and parking lot between Buildings 1 and 2.
  - d. A Stormwater Management Easement, subject to review and approval by the City Engineer and City Attorney, shall be recorded over the portions of the regional detention ponds that are located on the subject property.
  - e. The draft Declaration of Easements, Covenants, Conditions and Restrictions shall be finalized and, subject to approval by the City Attorney, shall be recorded after conditions 3.a, 3.b and 3.c have been met.
4. Extend the multi-use trail to School Street within five (5) years of approval of the PUD. The City acknowledges that this work may be subject to approval of adjacent property owners, as a result of which additional time has been allotted for completion of the trail.













February 19, 2019

Ms. Kelley Chrissie  
Assistant Community Development Director  
City of Wood Dale  
404 N. Wood Dale Road  
Wood Dale, IL 60191

Re: Redevelopment of 700 N. Wood Dale Road, Wood Dale, IL

Dear Ms. Chrissie:

Bridge Industrial Acquisition LLC ("Bridge") has entered into a "Contribution Agreement" with the owner of the property located at 700 N. Wood Dale Road in Wood Dale, which is commonly known as the Oakwood Commerce Center. The owner is Wood Dale Road Investors, LLC and is referred to as "Contributor" in the agreement while Bridge is referred to as "Developer". The property is located within an industrial business park and is surrounded by industrial buildings to the north and west, a lake to the south and the Wood Dale Fire Department, the Wood Dale Park District, Franzen Grove Park and the Wood Dale Junior High to the east across Wood Dale Road. The 21.5 acre site is currently improved with three single story office/flex buildings constructed in 1987 and totaling 240,000 square feet. The functionally obsolete buildings have been vacant for a number of years, which is why Bridge is seeking approval from the City of Wood Dale to redevelop the site with two, high end, state of the art, office and light industrial facilities comprising a total of 342,266 square feet per the documents in the attached applications. This proposed redevelopment will bring new jobs to Wood Dale, allow growing companies to stay in Wood Dale, add sales tax revenue and increase the real estate tax base.

The south building (Lot 2) is a proposed 100,378 square foot build to suit headquarters facility for Forward Space, LLC, a provider of high end office furniture and equipment. The precast concrete, glass and steel building will be 32' clear, contain 20,000 square feet of office and provide 183 car parking spaces for employees.

The north building (Lot 1) is a proposed 241,888 square foot, 32' clear, quasi spec, precast concrete, glass and steel industrial facility. It was designed to meet a specific users need but the user is delaying a decision and since Forward Space needs to occupy their building by Spring of 2020 and does not want construction occurring after they move in, Bridge is proposing the north building as a flexible build to suit/spec building. Wood Dale met with the potential user (User 1), who is a Wood Dale business looking to consolidate two smaller facilities into one. Bridge is still working with User 1 and is hopeful that they will commit to the site even though deviations in the TCC Wood Dale Road sub-area would be needed. If Lot 1 is developed as spec, the tenants will most likely be regional headquarters type, high office, light manufacturing, warehouse and distribution users requiring the same deviations. If User 1 commits, they will require 20,000 square feet of two story office and 175 car parking spaces.

The current plan is to conclude the City approval process by late March or early April 2019 so that demolition of the existing building can start in April and construction of the two proposed facilities can begin in June 2019 and conclude with occupancy for Forward Space in Lot 2 in March of 2020.

In requesting approval of the proposed redevelopment, Bridge is seeking deviations for the following items:

The TCC Wood Dale Road sub-area calls for a 100 foot parking setback on all sides of the property. The proposed developments of Lots 1 & 2 cannot meet this requirement and still provide the facility that Forward Space is requesting as well as the identified user for Lot 1. The proposed redevelopment would not be physically or economically feasible if it were to meet the 100 foot setback requirement. In lieu of the setback, Bridge is proposing that the 50 foot greenway be heavily landscaped where possible to help screen the parking areas for Lots 1 and 2. It also appears the 100 foot setback was intended for higher density office and residential uses that were contemplated for the site since the setback makes it virtually impossible to redevelop the site as presented.

The proposed build to suit for Forward Space on Lot 2 is a permitted use under the TCC Wood Dale Road sub-area but the building proposed for Lot 2 is not since it includes warehouse and distribution. In order to accommodate User 1 or any other prospective user, a deviation will be required to allow the warehouse/distribution use, which is currently allowed in the underlying



zoning of I-1, Light Industrial. This type of facility is needed in the area in order to attract new businesses to Wood Dale and keep existing businesses that are growing and have no place to go within Wood Dale.

Given that Forward Space requires a 20% office build out and User 1 requires an 8% office build out and any prospective tenant for Lot 1, Bridge is requesting a deviation from the current Office land use to a more appropriate Business Park land use, which will allow more parking flexibility. In addition, Lot 1 will require a parking deviation. The site plan currently identifies 175 spaces for Lot 1, while the zoning calls for 363. There are also an additional 37 spaces on the south end of the shared lot that may be available since Lot 2 is overparked. User 1, who would occupy the entire building, only requires 175 spaces. If User 1 does not occupy Lot 1, a typical user for Lot 1 will most likely require a 5% to 10% office build out with 90% to 95% warehouse/distribution. Given a standard parking ratio of 3 to 4 per 1,000 square feet of office space and 1 per 2,000 square feet of warehouse space, the 175 car spaces for Lot 1 plus the additional shared parking should be sufficient. If a user has heavier office or warehouse employee needs, they typically need less truck docks, so sections of the dock area can easily be stripped in order to accommodate up to 106 more car spaces.

Wood Dale's ordinance states that an access drive should be no more than 44 feet wide at the mouth and 33 feet wide at the right of way line. This appears to be directed more towards car traffic. Bridge's experience shows that fire trucks and semi-trucks needs significantly more in order to allow in bound and out bound traffic. The plans submitted show varying widths depending on the location and truck maneuvering needs. The requested deviation at the mouth is up to 110 feet and the requested deviation at the right of way line is up to 52 feet. Bridge is requesting a deviation for these dimensions to ensure the efficient flow and safety of vehicular traffic to and from the development.

The proposed fire hydrant spacing for this project does not uniformly meet the Wood Dale requirement of 300-foot hydrant spacing. Subject to fire flow tests that are currently being conducted, Bridge is requesting a deviation to allow for 600-foot spacing, which meets the IFC requirements.

Due to the request to make the shared parking lot a fire lane, the width of the drive aisle needs to be extended two feet, which means the building on Lot 2 shifts two feet to the south. In order to maintain the size of the patio, Bridge

is requesting a deviation to allow the patio to extend into the 50 foot greenway at the south end of Lot 2. Given that the the pond is located to the south of Lot 2, there is already a large natural buffer in the form of the pond so there will be no detrimental affect on any neighboring properties.

A deviation is requested to allow an administrative variance of up to 40% of the tree replacement requirements due to the limited space available on-site to properly provide replacement trees. Providing the replacement trees at full ordinance requirements would result in an improper planting approach and unaesthetic design. Per discussion with City staff, trees removed for the following reasons will not count towards the replacement number:

- Trees located in the newly proposed right of way
- Trees removed due to the multi-use path
- Trees currently located within landscaped islands

A deviation or administrative variance will also be requested due to the inability to plant street trees along East Pond Ave due to existing utility conflicts.

We at Bridge appreciate your consideration of this project and look forward to the opportunity of working with Wood Dale to its successful conclusion.

Sincerely,

A handwritten signature in black ink, appearing to read "Mark Houser".

Mark Houser  
Senior Vice President, Development

## Special Use Standards

No special use shall be authorized by the City Council unless the Community Development Commission shall find evidence establishing the following general standards and criteria (*found in Chapter 17, Article IV, Section 403.C of the Municipal Code*):

1. The proposed special use at the particular location requested is necessary or desirable to provide a service or a facility which is in the interest of the public and will contribute to the general welfare of the neighborhood or community;

**The proposed redevelopment of the site, whose current buildings have been vacant for years, will help revitalize the area and contribute to the welfare of the area by bringing new jobs, additional sales tax revenue and increased real estate tax revenue.**

2. The proposed special use will not have a substantial adverse effect upon the adjacent property, the character of the neighborhood, traffic conditions, utility facilities and other matters affecting the public health, safety and general welfare;

**The proposed redevelopment will have no adverse effect on the area and will only enhance the area by removing buildings that have been vacant for years. The new buildings will bring additional jobs to the area and have less of an overall impact on traffic and services than the previous buildings when they were occupied.**

3. The proposed special use will not be injurious to the use and enjoyment of other property in the immediate vicinity of the subject property for the purposes already permitted in such zoning district, nor substantially diminish and impair other property valuations within the neighborhood;

**The proposed redevelopment of the site is in keeping with the surrounding properties and will enhance the value of the area by removing long vacant and obsolete buildings on the site and bringing new job opportunities to the area.**

4. The nature, location, and size of buildings or structures involved with the establishment of the special use will not impede, substantially hinder, or discourage the development and use of adjacent land and buildings in accord with the zoning district within which they lie;

**The nature, location and size of the proposed redevelopment of the site is in keeping with the surrounding uses and will not adversely affect future development in the area.**

5. The proposed special use will be designed, arranged and operated so as to permit the development and use of neighboring property in accordance with the applicable district regulations;

**The proposed redevelopment of the site for office and light industrial use will have a positive effect on the development and use of neighboring properties by attracting new businesses and jobs to the area.**

6. Adequate utilities, access roads, drainage, and/or other necessary facilities have been or will be provided;

**The current infrastructure is more than adequate to accommodate the proposed development given that the site was previously developed with a use that in general had a higher demand on services.**

7. Parking areas shall be of adequate size for the particular special use, which areas shall be properly located and suitably screened from adjoining residential uses, and the entrance and exit driveways to and from these parking areas shall be designed so as to prevent traffic hazards, eliminate nuisance, and minimize traffic congestion in the public streets; and

**Parking for the proposed redevelopment will be adequate for the intended users and will be sufficiently screened by a heavily landscaped 50 foot greenway buffer surrounding the property. There are no adjoining residential uses and given the nature of the previous development on the site, there will be no additional impact on traffic congestion as compared to when the existing building were in use.**

8. Such other standards and criteria as are established by the ordinance for a particular special use as set forth in subsections D and H of Chapter 17 Article IV Section 403 of the Municipal Code , if applicable, and as applied to planned unit developments as set forth in Chapter 17, Article IV, Section 17.405 of the Municipal Code.

## **PUD Standards**

No planned unit development shall be authorized by the City Council unless the Community Development Commission shall find evidence establishing the following general standards and criteria (*found in Chapter 17, Article IV, Section 405.D of the Municipal Code*):

1. The proposed development will not injure or damage the use, value and enjoyment of surrounding property nor hinder or prevent the development of surrounding property in accordance with the Wood Dale comprehensive plan.

**The proposed redevelopment will not have an adverse effect on the surrounding properties and will only enhance their values by removing a long vacant complex of buildings and creating new job opportunities and bringing new sales tax revenues and increasing the real estate tax base. The planned development is in keeping with the vast majority of property in the surrounding area.**

2. The proposed development can be substantially completed within the period of time specified in the schedule of development submitted by the applicant.

**The proposed redevelopment of the site will be substantially completed by April of 2020. The south building will be fully occupied by Forward Space and the shell of the north building will substantially completed by January of 2020, ready for tenant build outs.**

3. The entire tract or parcel of land to be occupied by the proposed development shall be held in a single ownership, or if there are two (2) or more owners, the application for such proposed development shall be filed jointly by all such owners.

**The entire parcel will be owned by one entity with the ability to sell off the two buildings to separate owner/users if so desired.**

4. The development plan shall contain such proposed covenants, easements and other provisions relating to the bulk and location of buildings, uses and structures and public facilities as are necessary for the welfare of the planned unit development and are not inconsistent with the best interests of the city. Such covenants, easements and other provisions, when part of the approved final development, may be modified, removed or released only with the consent of the city council after a public hearing before, and recommendation by the community development commission as provided in this section.

**The proposed redevelopment will be governed by a newly created Declaration of Easements, Covenants, Conditions and Restrictions, which is included with the submittal package and is subject to City approval.**

5. Sanitary sewers, storm sewers and water supply to service the development are adequate to serve the proposed development and will not reduce existing capacity below that necessary to serve existing developments, or overload local facilities beyond design capacity.

**Since the site was previously developed and served by sanitary, storm and water, those existing services will meet the need of the proposed redevelopment and not reduce the capacities available for existing developments.**

6. The location and arrangement of structures, parking areas, walks, lighting and appurtenant facilities is compatible with the surrounding land uses, and any part of a proposed development not used for structures, parking and loading areas, or access ways, is landscaped or otherwise improved.

**The proposed redevelopment is compatible with the surrounding land uses and will have a heavily landscaped 50 foot greenway totally surrounding the site along with sidewalks and bike paths connecting to adjoining properties.**

7. The project area is adaptable to unified development and shall have within or through the area no physical features that will tend to destroy the neighborhood or community cohesiveness. There is no minimum project area for a planned unit development.

**The proposed redevelopment be very compatible with the surrounding area, which was predominantly developed as a business park in the 1980's.**

8. The uses permitted in the development are necessary or desirable and the need for such uses is clearly demonstrated by the applicant.

**The deviations requested for the proposed redevelopment are necessary in order to accommodate a full building user (Forward Space) who desires to relocate their corporate headquarters to Lot 2 of the development. The deviations are also needed to provide much needed space on Lot 1 for area businesses to grow without having to move from Wood Dale as well as attract new businesses to the area.**

9. The dominant land use of the proposed planned unit development is consistent with the recommendations of the Wood Dale comprehensive plan for the area containing the project.

**The proposed redevelopment is consistent with the goal of the comprehensive plan in that it eliminates a vacant, obsolete building complex and replaces it with a new, high end corporate headquarters with a heavy office build out and a high end speculative industrial building that will help Wood Dale maintain existing businesses that need more space and flexibility in a modern facility.**

10. Any modifications of the standards and specifications of this chapter or other regulations that would otherwise be applicable to the site are warranted by the design of the development plan, and the amenities incorporated in it, and are not inconsistent with the public general welfare.

**The modifications are warranted in order to accommodate buildings that will attract high end users such as Forward Space and allow growing businesses to stay in the area while in no way detracting from the public general welfare.**

11. Exceptional landscaping features such as larger caliper, varied species and reduced spacing of trees and additional sodding above the minimum requirements specified in section 17.606 of this chapter is provided.

**A detailed landscape plan is included in the submittal for approval, which maintains a 50 foot, heavily landscaped greenway around the entire site.**

12. All proposed streets and driveways are adequate to serve the residents, occupants, visitors or other anticipated traffic of the planned unit development. Entrance points or locations of streets

and driveways upon existing public roadways shall be subject to the approval of the city, and if applicable, the DuPage County highway department, and the Illinois department of transportation. If traffic control devices are required to prevent or relieve hazards or congestion on adjacent streets and the proposed control device is not within the normal or scheduled sequence of installations, the city council may require, as a condition of approval of a proposed planned unit development, such devices to be provided at the developer's cost.

**The proposed redevelopment is using the existing entrances on Mittel and Pond Drives to service the project and shifting the existing Wood Dale Road entrance to the south to line up with shared parking between Lot 1 and Lot 2. The DuPage County Highway Department has reviewed the request on Wood Dale Road and does not appear to have an issue with the relocation.**

13. Off street parking is conveniently accessible to all dwelling units and other uses in the planned unit development. Where appropriate, common driveways, parking areas, walks, and steps to parking and service areas are to be screened through ample use of trees, shrubs, hedges, landforms and walls.

**All off-site parking for the proposed buildings is conveniently accessible and the entire site is surrounded by a 50 foot, heavily landscaped greenway.**

14. A pedestrian circulation network is provided.

**An extensive pedestrian network is provided within the proposed redevelopment including sidewalks along Pond Drive, Mittel Drive and a bike path along Wood Dale Road that will provide easy access to the proposed buildings as well as area amenities.**

15. The planned unit development provides for underground installation of utilities (including electricity and telecommunications) in public ways and private extensions thereof. Provisions shall be made for acceptable design and construction of storm sewer facilities including grading, gutter, piping and treatment of turf to handle stormwater, prevent erosion and the formation of dust. Utilities and maintenance of facilities shall be in accordance with the requirements and regulations of the city as set forth in this chapter.

**All utilities are provided to the proposed buildings in accordance with Wood Dale regulations and requirements.**

16. The proposed planned unit development satisfies the applicable objectives as provided on page 9 of this packet.

**The site was previously fully developed with an office/flex building complex. The now functionally obsolete development has been vacant for a number of years which is why Bridge is proposing to redevelop the property with two, aesthetically pleasing, state of the**

art, office and light industrial facilities. Due to the fact that the site was previously developed, there are currently no existing archaeological and historic features on the site. The proposed redevelopment will preserve the existing natural landscape features to the greatest extent possible. The facilities will be surrounded by a heavily landscaped greenway and contain sidewalks and bike paths allowing for easy access to the lake at the south end of the property as well as other amenities within the neighborhood.

17. Existing ponds, creeks, rivers, lakes, wetlands or fens on or adjacent to the planned unit development are enhanced and protected from development.

**A new detention basin in lieu of the existing one is being provided per code. The two remaining basins on or adjacent to the site will be preserved and enhanced via native plantings. The basins will be protected from development in perpetuity and include a detailed maintenance plan to be implemented by the owners association.**

### **PUD Objectives**

The Planned Unit Development is intended to encourage improved design in the development of land by providing relief from traditional zoning requirements which are designed for conventional development but which may cause undue hardship or complication for desirable but unconventional development, and to establish standards and procedures for the issuance of a special use permit for a planned unit development in order to obtain the following objectives (*found in Chapter 17, Article IV, Section 405.D of the Municipal Code*):

1. Environmental design in the development of land that is of a higher quality than is normally possible through the strict application of general zoning ordinance requirements.

**Not applicable.**

2. Diversification in the use permitted and variation in the relationship of uses, structures, open spaces and heights of structures in developments conceived as cohesive unified projects.

**Not applicable.**

3. Provision for functional and beneficial use of open space.

**Sidewalks and bike paths will surround the development allowing for easy access to the lake at the south end of the property.**



4. Preservation, to the greatest extent possible, of the archaeological and historic resources and natural landscape features and amenities of a development site and to utilize such features in a harmonious fashion in the development.

**The site was previously fully developed with an office/flex complex and there are currently no existing archaeological and historic features on the site. The proposed redevelopment will preserve the existing natural landscape features to the greatest extent possible and enhance the property by providing a heavily landscaped greenway with sidewalks and bike paths for easy access to the lake at the south end of the property as well as other amenities nearby.**

5. Provision for a safe and desirable environment characterized by a sensitive and unified building and site development program.

**The proposed redevelopment will feature very high end, aesthetically pleasing elevations and well defined sidewalks, bike paths and ingress/egress for the safe flow of vehicular and pedestrian movements.**

6. Rational and economic development in relation to public services.

**Not applicable.**

7. Creation of a variety of uses, in compatible arrangements, to provide a greater choice of living, employment and shopping environments.

**Not applicable.**

8. Efficient use of land resulting in more economic networks of utilities, streets and other facilities.

**Not applicable.**

9. Coordination of architectural styles, building forms and relationships, graphics and other private improvements.

**Not applicable.**

## **Standards for Major Site Plan Review**

The Community Development Commission shall recommend approval and the City Council shall approve the Major Site Plan if the following standards are met:

1. The relationship of the site plan is consistent with the policies, goals and objectives of the comprehensive plan;

**The site plan is consistent with the comprehensive plan and will generate additional tax revenue and create new job opportunities for area residents.**

2. The proposed traffic and parking layout minimizes the danger and conflicts between pedestrians and motorists, provides efficient and convenient movement of traffic not only within the site but on adjacent roadways, and otherwise complies with the requirements of the Unified Development Ordinance (UDO), Chapter 17 of the Municipal Code;

**The proposed redevelopment provides sidewalks, bike paths and well situated ingress/egress points so as to minimize conflicts between pedestrians and motorists. Given the access points, the site also provides efficient and convenient access to area roadways.**

3. The location of principal structures, accessory structures and freestanding signs do not impede safe and efficient traffic circulation, stormwater drainage, or otherwise adversely impact adjoining land improvements;

**The proposed redevelopment will not impede safe and efficient traffic circulation, stormwater drainage or adversely impact adjoining properties.**

4. That the proposed use(s) is (are) permitted in the district in which the property is located;

**Lot 2 (south building) will be a permitted use, however Lot 1 (north building), which meets the underlying I-1 zoning, will need deviations to meet the TCC Overlay requirements. Lot 1 has been designed to facilitate the relocation and consolidation of an existing Wood Dale business. The said business (User 1) is seriously interested in Lot 1 but as of this date, has not been able to commit. Given that the user for Lot 2, Forward Space, has an aggressive move in schedule and was insistent that no construction occur on the site once they move in, Lot 1 is designed to be constructed simultaneously as either a build to suit for User 1 if they commit, or a high end speculative facility.**

5. That the proposed arrangement of buildings, off street parking, access, lighting, landscaping, and drainage is compatible with adjacent land uses and employs sound site planning principles; and

**All aspects of the proposed redevelopment are compatible with adjacent land uses and employ sound site planning principles.**

6. That all outdoor storage areas are screened and are in accordance with standards specified by the UDO.

**There are no current plans for outside storage. If future users request outside storage, it will be done in accordance with the UDO standards.**

## **Standards for Final Plat**

The Community Development Commission shall recommend approval and the City Council shall approve a Final Plat unless it makes written findings specifying the manner in which:

- a. The design and layout of the subdivision does not conform to the provisions of the UDO.

**The design and layout of the proposed redevelopment of the site does conform to the provisions of the UDO.**

- b. The applicant has not made adequate provision to install improvements required by the Community Development Commission or City Council under authority of the UDO.

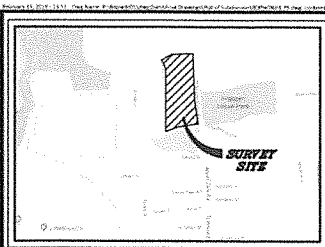
**The applicant has made adequate provisions to install improvements required by the Community Development Commission and City Council under authority of the UDO.**

- c. The Final Plat fails to comply with an approved Preliminary Plat.

**Bridge is applying for a combined Concept, Preliminary and Final Plat and as such complies with the standards for the Final Plat.**

- d. The Plat does not conform to the Comprehensive Plan, the Official Zoning Map, Article IV of Chapter 17 (UDO), City ordinances, or established planning policies of the City.

**The Plat does conform to the Comprehensive Plan, the Official zoning Map, Article IV of Chapter 17 (UDO), City ordinances and established planning policies of the City.**



# FINAL PLAT OF SUBDIVISION OF OAKWOOD COMMERCE CENTRE 2ND RESUBDIVISION BRIDGE DEVELOPMENT PARTNERS ADDITION TO WOOD DALE

A SUBDIVISION OF PART OF THE SOUTH HALF OF SECTION 4, TOGETHER WITH THE NORTH HALF OF SECTION 6, TOWNSHIP 40 NORTH, RANGE 11 EAST OF THE THIRD PRINCIPAL MERIDIAN, IN SULLY COUNTY, ILLINOIS.

GRAPHIC SCALE



**LOCATION MAP**

NOT TO SCALE

**OWNER/DEVELOPER**

BRIDGE DEVELOPMENT PARTNERS, LLC  
1000 W. IRVING PARK ROAD, SUITE 100  
ITASCA, IL 60143

**SURVEYOR, ENGINEER & LAND PLANNER**

MANHARD CONSULTING, LTD.  
700 SPRINGER DRIVE  
LOMBARD, IL 60148

**ARCHITECT**

CORNERSTONE ARCHITECTS LTD.  
1152 SPRING LAKE DRIVE  
ITASCA, IL 60143

**BASIS OF BEARINGS**

COORDINATES AND BEARINGS ARE BASED UPON THE ILLINOIS STATE PLANE COORDINATE SYSTEM, EAST ZONE (NAD 83), ADJUSTED TO GROUND VALUES, AS ESTABLISHED BY A REAL-TIME KINEMATIC (RTK) GLOBAL NAVIGATION SATELLITE SYSTEM (GNSS) UTILIZING THE TRIMBLE VISION NETWORK.

**CURRENT P.I.N.'S**

03-09-205-020  
03-09-205-025  
03-09-205-026  
03-09-205-027

**AREA SUMMARY**

LOT 1: 365,141 SQUARE FEET (8,297 AC)  
LOT 2: 385,898 SQUARE FEET (8,846 AC)  
BLW: 14,310 SQUARE FEET (3.28 AC)  
TOTAL: 765,349 SQUARE FEET (17,429 AC)

**SUBDIVISION DATA**

CURRENT ZONING: WOOD DALE ROAD SUB-AREA OF THE HYDRODALE CORRIDOR COORDINATE (HCC) WITH UNDERLYING I-1, LIGHT INDUSTRIAL. PROPOSED ZONING: SAME AS CURRENT ZONING.  
4 EXISTING LOTS  
2 PROPOSED LOTS  
SITE AREA: 21.82 ACRES  
MINIMUM LOT SIZE (ZONING): 3 ACRES  
MINIMUM LOT SIZE (PROPOSED): 8.5 ACRES

**LEGEND**

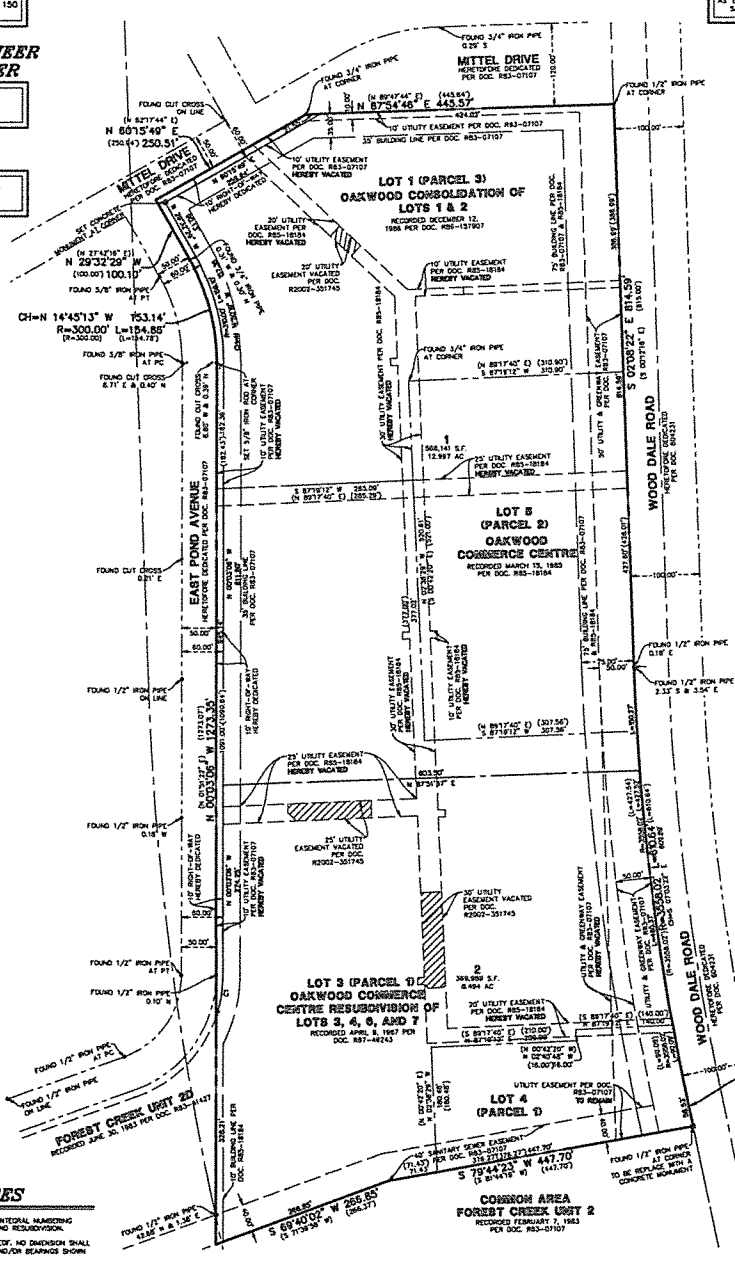
- EX PROPERTY LINE
- EXISTENT LINE
- EX SECTION LINE
- EX LOT LINE
- PROP. LOT LINE
- XXXLX --- MEASURED INFORMATION
- (XXX.XX) --- RECORD INFORMATION

**SHEET INDEX**

SHEET 1 OF 3	BOUNDARY, EX. EASEMENT, LOT DETAIL, AND SETBACK INFORMATION.
SHEET 2 OF 3	PROPOSED EASEMENT GRANT.
SHEET 3 OF 3	LEGAL DESCRIPTION, PROVISION LANGUAGE AND COPIES.

**CURVE TABLE**

CURVE	RADIUS	LENGTH	CHORD BEARING	CHORD
C1	185.00'	81.11'	S 69°43'30" W	80.83'



**SURVEYOR'S NOTES**

- THIS SUBDIVISION CONSISTS OF 2 LOTS AND IS PART OF AN INTERNAL MARKING SYSTEM TO EMBRACE ALL OF OAKWOOD COMMERCE CENTRE 2ND RESUBDIVISION.
- DISTANCES ARE MARKED IN FEET AND DECIMAL PLACES THEREOF. NO DIMENSION SHALL BE ASSUMED BY SCALE MEASUREMENTS. HORIZONTAL DISTANCES AND/OR BEARINGS SHOWN IN PARENTHESES (458.87) ARE RECORD OR DEED VALUES.
- THIS SUBDIVISION MAY BE SUBJECT TO MATTERS OF TITLE, WHICH MAY BE REVEALED BY A CURRENT TITLE REPORT. PRE-CLOSING EASEMENTS, RIGHTS AND OTHER RESTRICTIONS SHOULD BE FOUND IN A CURRENT TITLE REPORT. LOCAL ORDINANCES, DEEDS OR OTHER INSTRUMENTS OF RECORD MAY NOT BE SHOWN.
- Ø - DIMOTES CONCRETE MONUMENTS TO BE SET.
- IN ACCORDANCE WITH CHAPTER 165 ILCS SECTION 20-0.1, 2.1/2" X 1/2" LONG IRON RODS WILL BE SET AT ALL PROPERTY CORNERS AND POINTS OF GEOMETRIC CHANGE.
- UTILITY EASEMENT AND ANY OTHER EASEMENTS SHALL BE GRANTED BY A PLAT OR GRANT RECORDED AS A SEPARATE DOCUMENT.
- CITY ORDINANCES SUPERSEDE ANY PRIVATE COVENANTS AND RESTRICTIONS.

MANHARD CONSULTING, LTD. ALL RIGHTS RESERVED

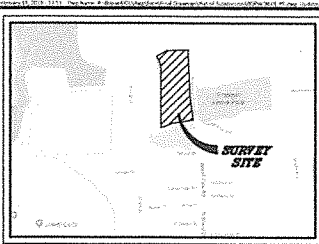
OAKWOOD COMMERCE CENTRE 2ND RESUBDIVISION
CITY OF WOOD DALE, ILLINOIS
FINAL PLAT OF SUBDIVISION

**Manhard CONSULTING**

700 Springer Drive, Lombard, IL 60148 | P: 630.491.8100 | F: 630.491.3566 | manhard.com

Civil Engineers | Surveyors | Utility Maintenance Engineers | Water & Wastewater Engineers

Construction Managers | Environmental Scientists | Landscape Architects | Planners

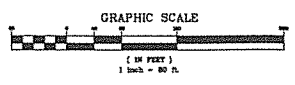


**LOCATION MAP**  
NOT TO SCALE

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BRIDGE DEVELOPMENT PARTNERS ADDITION TO WOOD DALE

A SUBDIVISION OF PART OF THE SOUTH HALF OF SECTION 4, TOGETHER WITH THE NORTH HALF OF SECTION 8, TOWNSHIP 40 NORTH, RANGE 11, EAST OF THE THIRD PRINCIPAL MERIDIAN, IN DUPAGE COUNTY, ILLINOIS.



**OWNER/DEVELOPER**

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1000 W. IRVING PARK ROAD, SUITE 150  
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**SURVEYOR, ENGINEER  
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**ARCHITECT**

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1152 SPRING LAKE DRIVE  
ITASCA, IL 60143

**BASIS OF BEARINGS**

COORDINATES AND BEARINGS ARE BASED UPON THE ILLINOIS STATE PLANE COORDINATE SYSTEM, EAST ZONE (NAD 83), ADJUSTED TO GRID VALUES, AS ESTABLISHED BY A REAL-TIME FORWARDING 1990 0206 NAVSTAR SATELLITE SYSTEM (RTS) UTILIZING THE TRIMBLE VRS HIGH NETWORK.

**CURRENT P.I.N.'S**

- 03-09-203-020
- 03-09-203-025
- 03-09-203-026
- 03-09-203-027

**AREA SUMMARY**

LOT 1: 566,141 SQUARE FEET (12.997 AC)  
LOT 2: 368,898 SQUARE FEET (8.464 AC)  
R.O.W.: 14,332 SQUARE FEET (3.283 AC)  
TOTAL: 950,480 SQUARE FEET (21.822 AC)

**SUBDIVISION DATA**

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MINIMUM LOT SIZE (PROPOSED): 0.5 ACRES

**LEGEND**

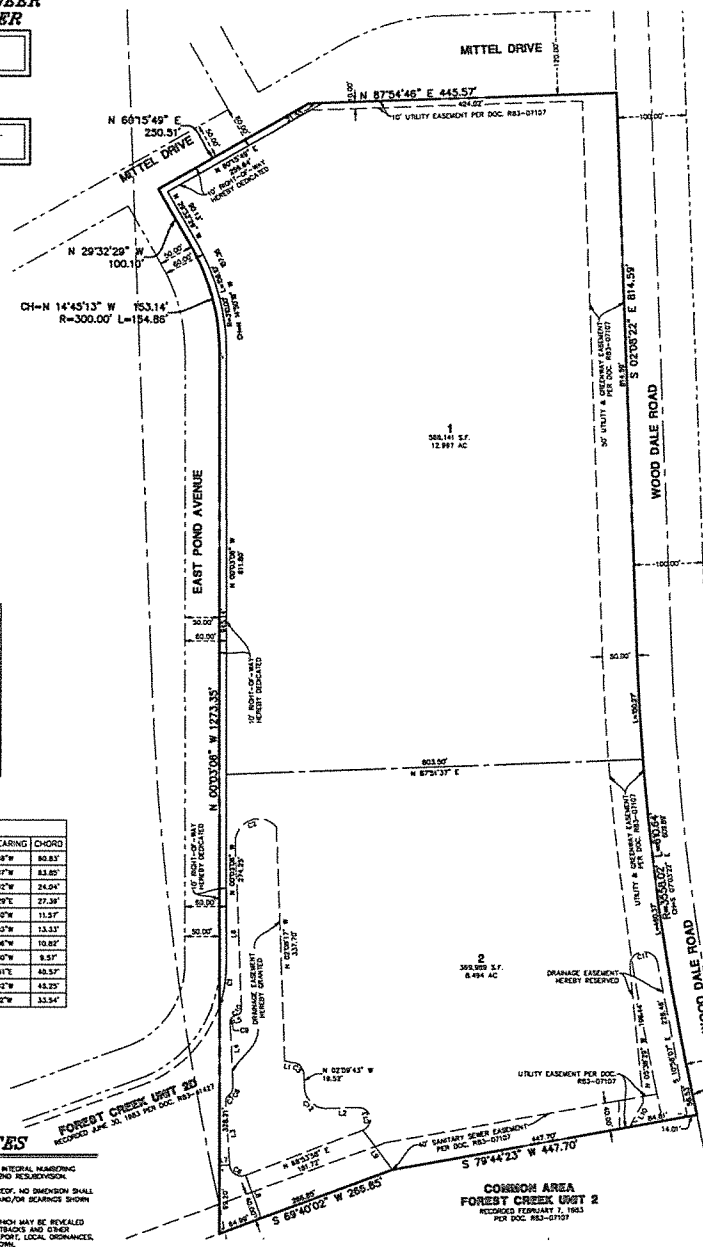
- - - - - EX. PROPERTY LINE
- - - - - EASEMENT LINE
- - - - - SETBACK LINE
- - - - - EX. SECTION LINE
- - - - - EX. LOT LINE
- - - - - PROP. LOT LINE
- (XXX) = MEASURED INFORMATION
- (XXX) = RECORD INFORMATION

LINE	BEARING	LENGTH
L1	N 87°54'46" E	18.52'
L2	S 27°12'32" W	44.31'
L3	S 02°16'30" E	80.88'
L4	S 02°33'10" E	93.14'
L5	S 71°18'21" W	5.69'
L6	S 02°17'21" E	240.09'
L7	S 08°24'34" W	13.02'
L8	S 29°19'38" E	66.00'
L9	N 83°14'25" W	71.34'
L10	N 82°46'34" E	40.72'

CURVE	RADIUS	LENGTH	CHORD BEARING	CHORD
C1	183.00'	61.11'	S 07°14'38" W	60.83'
C2	24.00'	82.83'	S 87°28'27" W	83.80'
C3	17.00'	28.70'	N 47°09'02" W	24.04'
C4	17.00'	31.84'	S 57°29'29" E	27.38'
C5	12.00'	10.00'	S 25°02'10" W	11.37'
C6	14.10'	13.88'	S 22°20'20" W	13.53'
C7	8.00'	11.80'	S 34°27'04" W	10.92'
C8	8.00'	10.38'	S 34°34'30" W	9.31'
C9	20.33'	61.35'	N 80°43'41" E	48.37'
C10	38.00'	80.35'	N 47°09'02" W	48.75'
C11	17.00'	47.78'	N 11°37'12" W	33.54'

**SURVEYOR'S NOTES**

- THIS SUBDIVISION CONSISTS OF 2 LOTS AND IS PART OF AN INTERNAL MARKETING SYSTEM TO DEVELOP ALL OF OAKWOOD COMMERCE CENTRE 2ND RESUBDIVISION.
- DISTANCES ARE SHOWN IN FEET AND DECIMAL PLACES THEREOF. NO DIMENSION SHALL BE ASSUMED BY SCALE MEASUREMENT HEREON. DISTANCES AND/OR BEARINGS SHOWN IN PARENTHESES (568.97) ARE RECORD OR BEED VALUES.
- THIS SUBDIVISION MAY BE SUBJECT TO MATTERS OF TITLE, WHICH MAY BE REVEALED BY A CURRENT TITLE REPORT, PRE-EXISTING EASEMENTS, RESTRICTIONS AND OTHER RESTRICTIONS WHICH MAY BE FOUND IN A CURRENT TITLE REPORT, LOCAL ORDINANCES, DEEDS OR OTHER INSTRUMENTS OF RECORD. THIS SHALL NOT BE SHOWN.
- Ø - SHOWS CONCRETE MONUMENTS TO BE SET.
- IN ACCORDANCE WITH CHAPTER 15.05 SECTION 15.05-1.2.1 LONG IRON WOOD SHALL BE SET AT ALL PROPERTY CORNERS AND POINTS OF GEOMETRIC CHANGE.
- UTILITY EASEMENT AND ANY OTHER EASEMENTS SHALL BE DRAWN BY A PLAT OR GRANT RECORDED AS A SEPARATE DOCUMENT.
- CITY ORDINANCES SUPERSEDE ANY PRIVATE COVENANTS AND RESTRICTIONS.



MANHARD CONSULTING, LTD. ALL RIGHTS RESERVED

**OAKWOOD COMMERCE CENTRE 2ND RESUBDIVISION**

CITY OF WOOD DALE, ILLINOIS

**FINAL PLAT OF SUBDIVISION**

2 OF 3

700 Spring Drive, Lombard, IL 60148 PH: 630.891.8200 FX: 630.891.2055 manhard.com  
Civil Engineer | Electrical | Water Resources Engineer | Planner & Vision | Utility Engineer  
Construction Manager | Environmental Scientist | Landscape Architect | Planner

DATE	REVISION	DRAWN BY
02/27/2018	REVISION FOR CITY COMMENTS	MS
02/27/2018	INCORPORATE CITY COMMENTS	MS

FINAL PLAT OF SUBDIVISION
OF
OAKWOOD COMMERCE CENTRE
2ND RESUBDIVISION

BRIDGE DEVELOPMENT PARTNERS ADDITION TO WOOD DALE
A SUBDIVISION OF PART OF THE SOUTH HALF OF SECTION 4, TOGETHER WITH THE NORTH HALF OF SECTION 6, TOWNSHIP 40 NORTH, RANGE 11, EAST OF THE
THIRD PRINCIPAL MERIDIAN, IN DUPAGE COUNTY, ILLINOIS.

OWNER'S CERTIFICATE

STATE OF ILLINOIS )
COUNTY OF DUPAGE ) S.S.

THIS IS TO CERTIFY THAT ... IS THE OWNER OF THE LAND DESCRIBED
IN THE FOREGOING SURVEYOR'S CERTIFICATE AND HAS CAUSED THE SAME TO BE SURVEYED,
RECORDED AND PLATTED AS SHOWN ON THE ANNEXED PLAT FOR THE USES AND PURPOSES
HEREIN SET FORTH AS ALLOWED AND PROVIDED BY STATUTE. THE SUBDIVISION TO BE KNOWN
AS "OAKWOOD COMMERCE CENTRE 2ND RESUBDIVISION", CITY OF WOOD DALE, DUPAGE COUNTY,
ILLINOIS AND DOES HEREBY ACKNOWLEDGE AND ADOPT SAME UNDER THE ADDRESSAS STYLE
AND TITLE.

DATED THIS ... DAY OF ... A.D. 20...

OWNER: \_\_\_\_\_

ATTEST: \_\_\_\_\_

NOTARY PUBLIC

STATE OF ILLINOIS )
COUNTY OF DUPAGE ) S.S.

I, AND FOR THE COUNTY AND STATE ADDRESSAS, DO HEREBY CERTIFY THAT I A NOTARY PUBLIC
... OF ... WHO IS PERSONALLY KNOWN TO ME
TO BE THE SAME WHOSE NAME(S) IS/ARE SUBSCRIBED TO THE FOREGOING CERTIFICATE,
APPEARED BEFORE ME THIS DAY IN PERSON AND ACKNOWLEDGED THAT HE/SHE/HIS/HER SON
AND DELIVER THIS INSTRUMENT AS A FREE AND VOLUNTARY ACT FOR THE USES AND
PURPOSES HEREIN SET FORTH.

GIVEN UNDER MY HAND AND NOTORIAL SEAL THIS ... DAY OF ... A.D. 20...

SCHOOL DISTRICT BOUNDARY STATEMENT

STATE OF ILLINOIS )
COUNTY OF DUPAGE ) S.S.

THE UNDERSIGNED DO HEREBY CERTIFY THAT, AS OWNERS OF THE PROPERTY DESCRIBED IN THE
SURVEYOR'S CERTIFICATE, AND KNOWN AS "OAKWOOD COMMERCE CENTRE 2ND RESUBDIVISION", TO
THE BEST OF THEIR KNOWLEDGE, IS LOCATED WITHIN THE BOUNDARIES OF THE WOOD DALE
SCHOOL DISTRICT 7 AND PAVEN HIGH SCHOOL DISTRICT 100 IN DUPAGE COUNTY, ILLINOIS.
DATED AT ... ILLINOIS THIS ... DAY OF ... A.D. 20...

DATED THIS ... DAY OF ... A.D. 20...

OWNER: \_\_\_\_\_

ATTEST: \_\_\_\_\_

CITY COUNCIL CERTIFICATE

STATE OF ILLINOIS )
COUNTY OF DUPAGE ) S.S.

PLAT APPROVED BY THE CITY COUNCIL OF THE CITY OF WOOD DALE
THIS ... DAY ... OF A.D. 20...

BY: MAYOR \_\_\_\_\_

ATTEST: CITY CLERK \_\_\_\_\_

CITY TREASURER CERTIFICATE

STATE OF ILLINOIS )
COUNTY OF DUPAGE ) S.S.

I, ... TREASURER FOR THE CITY OF WOOD DALE, DUPAGE
COUNTY, ILLINOIS, DO HEREBY CERTIFY THAT THERE ARE NO UNPAID CURRENT
OR FORFEITED SPECIAL ASSESSMENTS OR ANY DEFERRED INSTALLMENTS THEREOF THAT HAVE
BEEN ASSESSMENT AGAINST THE TRACT OF LAND INCLUDED ON THIS PLAT.
DATED THIS ... DAY ... OF A.D. 20...

BY: CITY TREASURER \_\_\_\_\_

COMMUNITY DEVELOPMENT COMMISSION CERTIFICATE

STATE OF ILLINOIS )
COUNTY OF DUPAGE ) S.S.

PLAT APPROVED BY THE COMMUNITY DEVELOPMENT COMMISSION OF THE CITY OF WOOD DALE
THIS ... DAY ... OF A.D. 20...

BY: COMMUNITY DEVELOPMENT COMMISSION CHAIRMAN \_\_\_\_\_

COUNTY HIGHWAY CERTIFICATE

STATE OF ILLINOIS )
COUNTY OF DUPAGE ) S.S.

THIS PLAT HAS BEEN APPROVED BY THE DUPAGE COUNTY HIGHWAY DEPARTMENT WITH
RESPECT TO ACCESS TO COUNTY HIGHWAY NO. 20. ALSO KNOWN AS WOOD DALE
ROAD, ... ILLINOIS. THE HIGHWAY DEPARTMENT HAS REVIEWED THE
PROPOSED PLAT AND HAS DETERMINED THAT THE PROPOSED PLAT IS IN ACCORDANCE WITH
THE REQUIREMENTS OF THE ILLINOIS HIGHWAY DEPARTMENT. HOWEVER, A HIGHWAY PERMIT FOR ACCESS IS REQUIRED OF THE OWNER OF THE
PROPERTY PRIOR TO CONSTRUCTION WITHIN THE COUNTY RIGHT-OF-WAY.

DATED THIS ... DAY OF ... A.D. 20...

BY: COUNTY ENGINEER \_\_\_\_\_

COUNTY CLERK'S CERTIFICATE

STATE OF ILLINOIS )
COUNTY OF DUPAGE ) S.S.

I, ... COUNTY CLERK OF DUPAGE COUNTY, ILLINOIS DO HEREBY CERTIFY THAT THERE
ARE NO UNPAID CURRENT TAXES, NO UNPAID CURRENT FINES OR SPECIAL ASSESSMENTS AND NO UNPAID FORFEITED
TAXES, AND NO RECEIVABLE TAX SALES AGAINST ANY OF THE LAND INCLUDED IN THE PLAT.
I FURTHER CERTIFY THAT I HAVE RECEIVED ALL STATUTORY FEES IN CONNECTION WITH THE PLAT.
GIVEN UNDER MY HAND AND SEAL OF THE COUNTY CLERK OF DUPAGE COUNTY, ILLINOIS THIS
... DAY OF ... A.D. 20...

COUNTY CLERK \_\_\_\_\_

DUPAGE COUNTY RECORDER'S CERTIFICATE

STATE OF ILLINOIS )
COUNTY OF DUPAGE ) S.S.

THIS INSTRUMENT NO. ... WAS FILED FOR RECORDING IN THE RECORDER'S OFFICE OF
DUPAGE COUNTY, ILLINOIS, ON THIS ... DAY OF ... A.D. 20...

BY: COUNTY RECORDER \_\_\_\_\_

SURFACE WATER STATEMENT

STATE OF ILLINOIS )
COUNTY OF DUPAGE ) S.S.

WE HEREBY CERTIFY THAT THE TOPOGRAPHICAL AND PROFILE STAKES REQUIRED BY THE ILLINOIS PLAT ACT,
ILLINOIS REGISTERED SURVEYOR CHAPTER 100, § 2.02 (2)(B), AS NOW OR HEREIN AMENDED, HAVE BEEN FILED WITH
THE CITY OF WOOD DALE, DUPAGE COUNTY, ILLINOIS, AND TO THE BEST OF OUR KNOWLEDGE AND BELIEF THE
SURFACE WATER DRAINAGE PATTERNS WILL NOT BE CHANGED BY THE CONSTRUCTION OF SUCH SUBDIVISION OR ANY
PART THEREOF, OR THAT IF SUCH SURFACE WATER DRAINAGE WILL BE CHANGED, REASONABLE PROVISIONS HAS
BEEN MADE FOR THE COLLECTION AND DISCHARGE OF SUCH SURFACE WATERS INTO PUBLIC WATERS OR SHALVE
WHICH THE SUBDIVIDER HAS A RIGHT TO USE, AND THAT SUCH SURFACE WATERS WILL BE PLANNED FOR IN
ACCORDANCE WITH GENERALLY ACCEPTED ENGINEERING PRACTICES SO AS TO REDUCE THE LIKELIHOOD OF DAMAGE
TO THE ADJACENT PROPERTY BECAUSE OF THE CONSTRUCTION OF THE SUBDIVISION.

DATED THIS ... DAY OF ... 20...

ILLINOIS REGISTERED PROFESSIONAL ENGINEER
ILLINOIS REGISTRATION NUMBER 083-026833
LICENSE EXPIRES NOVEMBER 30, 2019

OWNER: \_\_\_\_\_ OR ATTORNEY: \_\_\_\_\_

POSSIBLITY TO RECORD

STATE OF ILLINOIS )
COUNTY OF DUPAGE ) S.S.

I, SAMUEL J. PHILLIPS, AN ILLINOIS PROFESSIONAL LAND SURVEYOR, HEREBY GRANT PERMISSION TO A
REPRESENTATIVE OF ... TO RECORD THIS PLAT. THE REPRESENTATIVE SHALL
PROVIDE THIS SURVEYOR WITH A RECORDED COPY OF THIS PLAT.

DATED THIS ... DAY OF ... A.D. 20...

ILLINOIS PROFESSIONAL LAND SURVEYOR REG. 033-026899
LICENSE EXPIRES NOVEMBER 30, 2020

SURVEYOR'S CERTIFICATE

STATE OF ILLINOIS )
COUNTY OF DUPAGE ) S.S.

THIS IS TO CERTIFY THAT I, SAMUEL J. PHILLIPS, AN ILLINOIS PROFESSIONAL LAND SURVEYOR, HAVE SURVEYED AND
SUBDIVIDED AND PLATTED FOR THE OWNERS' USE OF THE FOLLOWING DESCRIBED PROPERTY:

PARCEL 1
LOT 3 AND 4 IN OAKWOOD COMMERCE CENTRE RESUBDIVISION OF LOTS 3, 4, 5, AND 7, BEING A RESUBDIVISION OF
LOTS 3, 4, 5, AND 7 OF OAKWOOD COMMERCE CENTRE SUBDIVISION, BEING A RESUBDIVISION OF LOTS 321 THROUGH 313
IN FOREST CREEK UNIT #2, BEING A SUBDIVISION OF PART OF THE SOUTH HALF OF SECTION 4, TOWNSHIP 40 NORTH, RANGE 11, EAST OF THE THIRD PRINCIPAL MERIDIAN, ACCORDING TO
THE PLAT OF SAID OAKWOOD COMMERCE CENTRE RESUBDIVISION OF LOTS 3, 4, 5, AND 7 RECORDED APRIL 9, 1997 AS
DOCUMENT NO. 887-98243, IN DUPAGE COUNTY, ILLINOIS.

PARCEL 2
A OAKWOOD COMMERCE CENTRE, BEING A RESUBDIVISION OF LOTS 201 THROUGH 213, BOTH INCLUSIVE, IN
FOREST CREEK UNIT #2, BEING A SUBDIVISION OF PART OF THE SOUTH HALF OF SECTION 4, TOWNSHIP 40 NORTH, RANGE 11, EAST OF THE THIRD PRINCIPAL MERIDIAN, ACCORDING TO
THE PLAT OF SAID OAKWOOD COMMERCE CENTRE RESUBDIVISION OF LOTS 3, 4, 5, AND 7 RECORDED APRIL 9, 1997 AS
DOCUMENT NO. 887-98243, IN DUPAGE COUNTY, ILLINOIS.

PARCEL 3
LOT 1 IN OAKWOOD CONSOLIDATION OF LOTS 1 AND 2, BEING A RESUBDIVISION OF LOTS 1 AND 2 IN OAKWOOD
COMMERCE CENTRE, BEING A SUBDIVISION OF THE NORTH HALF OF SECTION 6, ALL IN TOWNSHIP 40 NORTH, RANGE 11,
EAST OF THE THIRD PRINCIPAL MERIDIAN, ACCORDING TO THE PLAT OF SAID OAKWOOD CONSOLIDATION OF LOTS 1 AND
2 RECORDED DECEMBER 12, 1983 AS DOCUMENT NO. 888-17970, IN DUPAGE COUNTY, ILLINOIS.
CONTAINING 21.820 ACRES, MORE OR LESS.

I FURTHER CERTIFY THAT IRON STAKES HAVE BEEN SET AT ALL LOT CORNERS, POINTS OF CURVATURE AND TANGENCY,
EXCEPT WHERE CONCRETE MONUMENTS ARE INDICATED, AND THAT THE PLAT HEREON DRAWN CORRECTLY REPRESENTS
SAID SURVEY AND SUBDIVISION; ALL DIMENSIONS ARE GIVEN IN FEET AND DECIMAL PARTS THEREOF.
I FURTHER CERTIFY THAT THE FOREGOING PROPERTY FALLS WITHIN THE CORPORATE LIMITS OF THE CITY OF WOOD DALE,
AND I FURTHER CERTIFY THAT NO PART OF SAID PROPERTY IS SITUATED WITHIN A FLOOD HAZARD AREA, AS PER
NATIONAL FLOOD INSURANCE PROGRAM FLOOD INSURANCE RATE MAP, COMMUNITY PANEL NUMBER 170433020,
EFFECTIVE DATE OF DECEMBER 16, 2004.

DATED AT OAKWOOD, ILLINOIS, THIS ... DAY OF ... 20...

FOR REVIEW ONLY

ILLINOIS PROFESSIONAL LAND SURVEYOR REG. 033-026899
LICENSE EXPIRES NOVEMBER 30, 2020

SOCHI PAIN PROFESSIONAL LICENSE NO. 184003320
LICENSE EXPIRES APRIL 30, 2019

DATE OF FIELD SURVEY: JANUARY 21, 2019

THIS PROFESSIONAL SERVICE CONFORMS TO THE CURRENT ILLINOIS
MINIMUM STANDARDS FOR A BOUNDARY SURVEY.

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OAKWOOD COMMERCE CENTRE 2ND RESUBDIVISION
CITY OF WOOD DALE, ILLINOIS
FINAL PLAT OF SUBDIVISION

Manhard CONSULTING
750 Barringer Drive, Lombard, IL 60148 | 630.881.8500 | 630.881.8155 manhard.com
Civil Engineers | Surveyors | Interior Designers | Kitchen Designers | Construction Managers | Environmental Scientists | Landscape Architects | Planners

Table with columns: DATE, VERSION, SHEET, TOTAL SHEETS, and a grid for recording survey data.

The Architectural Site Plans & Elevations are too large to include in this file but are available at: <http://www.wooddale.com/Home/ShowDocument?id=5215>

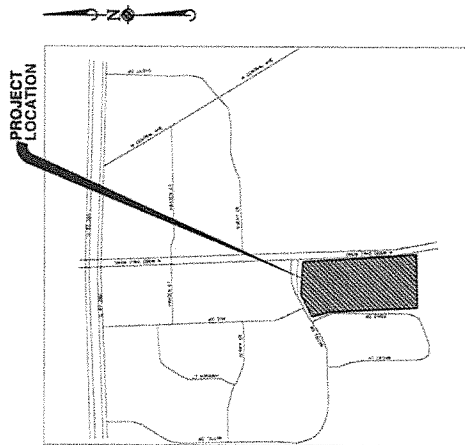
Preliminary Improvements for

# OAKWOOD COMMERCE CENTER REDEVELOPMENT

700 N WOOD DALE ROAD  
CITY OF WOOD DALE, ILLINOIS

### STANDARD SYMBOLS

EXISTING	PROPOSED
ASBESTOS	ASBESTOS
AVENUE	AVENUE
BRIDGE	BRIDGE
CONCRETE	CONCRETE
... (many more symbols and descriptions) ...	... (many more symbols and descriptions) ...



BRIDGE DEVELOPMENT PARTNERS, LLC  
1000 W. IRVING PARK ROAD, SUITE 150  
ITASCA, IL 60143  
312-683-7230



**Manhard CONSULTING LTD.**  
1400 Riverchase Drive, Suite 500, Wood Dale, IL 60190  
P: 312.683.7230 F: 312.683.7231  
www.manhardconsulting.com

### Sheet List Table

SHEET NO.	DESCRIPTION
1	TITLE SHEET
2	EXISTING CONDITIONS & DEMOLITION PLAN - NORTH
3	EXISTING CONDITIONS & DEMOLITION PLAN - SOUTH
4	OVERALL UTILITY PLAN
5	SITE DIMENSIONAL & PAVING PLAN - NORTH
6	SITE DIMENSIONAL & PAVING PLAN - SOUTH
7	GRADING PLAN - NORTH
8	GRADING PLAN - SOUTH
9	UTILITY PLAN - NORTH
10	UTILITY PLAN - SOUTH

**SHEETS TO BE ADDED FOR FINAL ENGINEERING SUBMITTAL**

INDEX OF SHEETS  
 GRADING DETAILS  
 SOIL EROSION & SEDIMENT CONTROL PLAN  
 CONSTRUCTION DETAILS  
 CONSTRUCTION DETAILS  
 CONSTRUCTION DETAILS  
 CONSTRUCTION SPECIFICATIONS



OAKWOOD COMMERCE CENTER REDEVELOPMENT  
CITY OF WOOD DALE, ILLINOIS  
TITLE SHEET  
1 of 10  
DATE: 11/20/13  
BY: [Signature]

**NOTES:**

- THE BOUNDARY LINES AND TOPOGRAPHY FOR THIS PROJECT ARE BASED ON A FIELD SURVEY COMPLETED BY MANHARD CONSULTING, L.L.C. ON OCTOBER 17, 2013. THE CONTRACTOR SHALL VERIFY THE EXISTING CONDITIONS AND TOPOGRAPHY IMMEDIATELY AFTER COMMENCING WORK AND NOTIFY MANHARD CONSULTING AND THE CLIENT IN WRITING OF ANY DIFFERING CONDITIONS.

**BENCHMARKS:**

REFERENCE BENCHMARK (NGS 860420) FOUND SURVEY MARK LOCATED AT THE INTERSECTION OF IRVING PARK ROAD AND WOOD DALE ROAD. DATUM=NAVD83-GLD00 129 ELEVATION=720.80

**SITE BENCHMARKS:**

CUT SIGN IN CONCRETE ON THE SW CORNER OF A TRAFFIC SIGNAL AT THE INTERSECTION OF WOOD DALE AND IRVING PARK. DATUM=NAVD83-GLD00 129 ELEVATION=711.02

**SITE BENCHMARKS:**

CONCRETE ON A FISC. HYDRANT AT THE SW CORNER OF THE NORTH ENTRANCE AND ON THE S SIDE OF IRVING PARK. DATUM=NAVD83-GLD00 129 ELEVATION=720.14



### ABBREVIATIONS

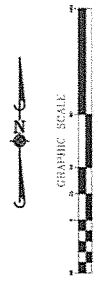
ASBESTOS	ASBESTOS
AVENUE	AVENUE
BRIDGE	BRIDGE
CONCRETE	CONCRETE
... (many more abbreviations) ...	... (many more abbreviations) ...








MANHARD CONSULTING, LTD. IS NOT RESPONSIBLE FOR THE SAFETY OF ANY PARTY AT OR ON THE CONSTRUCTION SITE. SAFETY IS THE SOLE RESPONSIBILITY OF THE CONTRACTOR AND ANY OTHER PERSON ON THE JOB SITE. SAFETY OF PERSONS ENGAGED IN THE WORK OR METHODS OF CONSTRUCTION FOR THE JOB SITE.



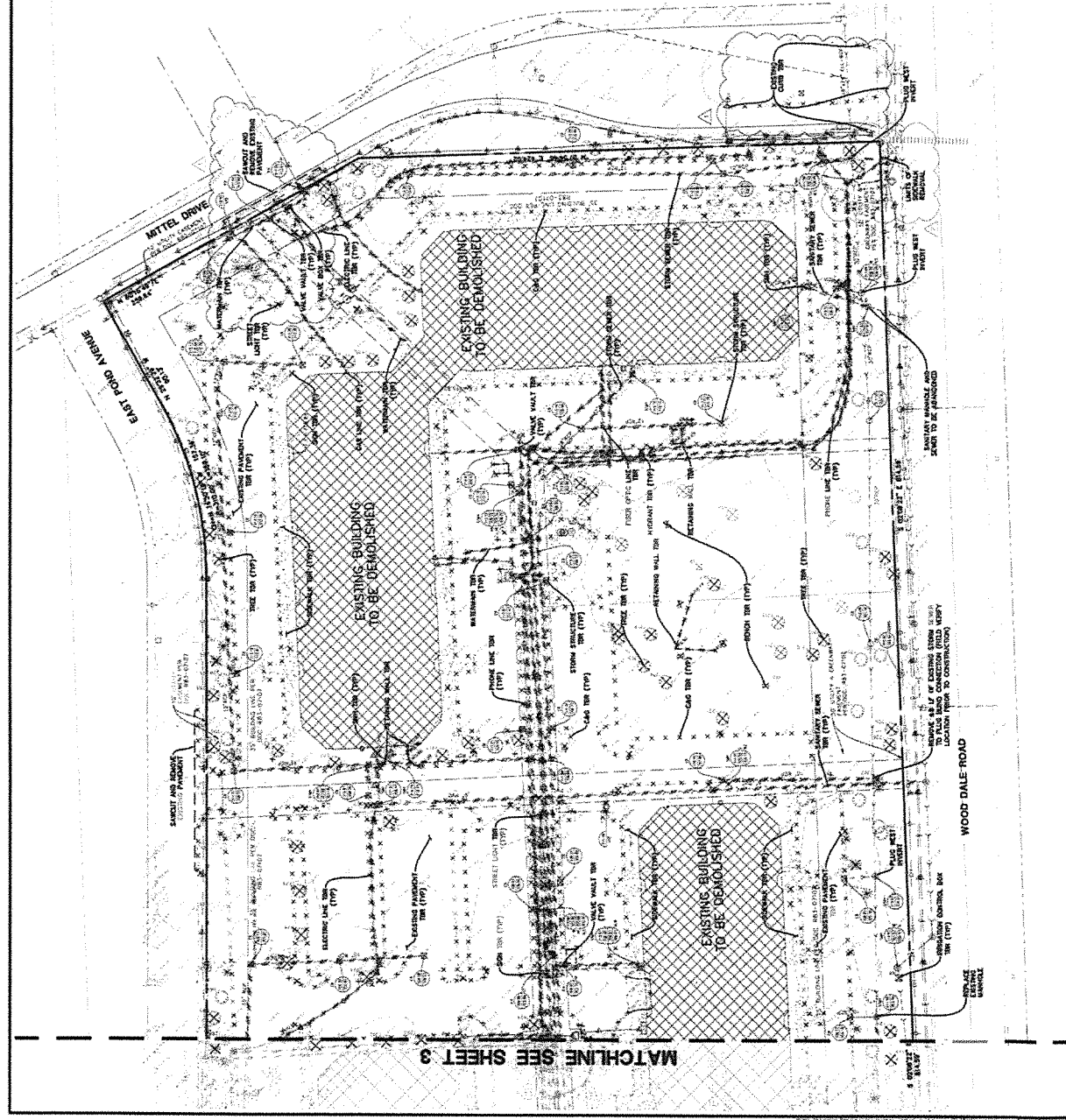
OAKWOOD COMMERCE CENTER REDEVELOPMENT  
 CITY OF WOOD DALE, ILLINOIS  
 EXISTING CONDITIONS & DEMOLITION PLAN - NORTH

2 OF 10  
 SHEET



DEMOLITION LEGEND	
	ALL BUILDINGS MARKED BY THE PROPERTY OWNER TO BE REMOVED UNLESS OTHERWISE NOTED
	EXISTING BUILDING TO BE REMOVED
	EXISTING FOUNDATION WALL, RAISED FLOOR, WALLS, CURB AND GUTTER, ETC. TO BE REMOVED
	RECYCLE STRUCTURE TO BE REMOVED
	RECYCLE FLOORING TO BE REMOVED
	NET PYS
	TO BE REMOVED

- #### GENERAL NOTES
- THE CONTRACTOR SHALL COORDINATE WITH ALL UTILITIES (WATER, GAS, ELECTRICITY, TELEPHONE, CABLE TELEVISION, FIBER OPTIC, AND SANITARY SEWER) TO BE REMOVED OR RELOCATED. THE CONTRACTOR SHALL BE RESPONSIBLE FOR OBTAINING ALL NECESSARY PERMITS FROM THE CITY OF WOOD DALE AND THE ILLINOIS DEPARTMENT OF TRANSPORTATION (IDOT) FOR ALL UTILITIES TO BE REMOVED OR RELOCATED.
  - ALL EXISTING UTILITIES TO BE REMOVED SHALL BE SHOWN ON THIS PLAN AND SHALL BE REMOVED AS SHOWN. ALL UTILITIES TO BE RELOCATED SHALL BE SHOWN ON THIS PLAN AND SHALL BE RELOCATED AS SHOWN.
  - ALL EXISTING FOUNDATION WALLS, RAISED FLOORS, WALLS, CURBS AND GUTTERS, AND OTHER STRUCTURES TO BE REMOVED SHALL BE SHOWN ON THIS PLAN AND SHALL BE REMOVED AS SHOWN.
  - ALL EXISTING RECYCLE STRUCTURES AND RECYCLE FLOORING TO BE REMOVED SHALL BE SHOWN ON THIS PLAN AND SHALL BE REMOVED AS SHOWN.
  - ALL EXISTING NET PYS TO BE REMOVED SHALL BE SHOWN ON THIS PLAN AND SHALL BE REMOVED AS SHOWN.
  - ALL EXISTING STRUCTURES TO BE REMOVED SHALL BE SHOWN ON THIS PLAN AND SHALL BE REMOVED AS SHOWN.
- #### PERMITTING REQUIREMENTS
- THE CONTRACTOR SHALL OBTAIN ALL NECESSARY PERMITS FROM THE CITY OF WOOD DALE AND THE ILLINOIS DEPARTMENT OF TRANSPORTATION (IDOT) FOR ALL UTILITIES TO BE REMOVED OR RELOCATED.
  - THE CONTRACTOR SHALL OBTAIN ALL NECESSARY PERMITS FROM THE CITY OF WOOD DALE AND THE ILLINOIS DEPARTMENT OF TRANSPORTATION (IDOT) FOR ALL UTILITIES TO BE RELOCATED.
  - THE CONTRACTOR SHALL OBTAIN ALL NECESSARY PERMITS FROM THE CITY OF WOOD DALE AND THE ILLINOIS DEPARTMENT OF TRANSPORTATION (IDOT) FOR ALL UTILITIES TO BE REMOVED OR RELOCATED.
  - THE CONTRACTOR SHALL OBTAIN ALL NECESSARY PERMITS FROM THE CITY OF WOOD DALE AND THE ILLINOIS DEPARTMENT OF TRANSPORTATION (IDOT) FOR ALL UTILITIES TO BE RELOCATED.



MATCHLINE SEE SHEET 3

**GRAPHIC SCALE:**  
 1" = 50' (0' 0" - 100' 0")

**CONSTRUCTION SHALL COMPLY WITH RESPECTIVE UTILITY**  
 REGULATIONS AND SHALL BE IN ACCORDANCE WITH THE CITY OF WOOD DALE  
 ORDINANCES. THE CONTRACTOR SHALL BE RESPONSIBLE FOR OBTAINING ALL  
 NECESSARY PERMITS AND APPROVALS FROM THE CITY OF WOOD DALE  
 AND THE ILLINOIS DEPARTMENT OF TRANSPORTATION (IDOT) PRIOR TO  
 CONSTRUCTION. THE CONTRACTOR SHALL BE RESPONSIBLE FOR PROTECTING  
 ALL UTILITIES AND STRUCTURES TO REMAIN. ALL UTILITIES SHALL BE  
 DEEPENED TO A MINIMUM OF 48" BELOW FINISH GRADE. ALL UTILITIES  
 SHALL BE COVERED WITH A MINIMUM OF 18" OF CONCRETE. ALL UTILITIES  
 SHALL BE MARKED WITH RED X'S AND IDENTIFIED BY THE CONTRACTOR.

**EXISTING CONDITIONS AND DEMOLITION PLAN - SOUTH**

1. THE CONTRACTOR SHALL COMPLY WITH RESPECTIVE UTILITY REGULATIONS AND SHALL BE IN ACCORDANCE WITH THE CITY OF WOOD DALE ORDINANCES. THE CONTRACTOR SHALL BE RESPONSIBLE FOR OBTAINING ALL NECESSARY PERMITS AND APPROVALS FROM THE CITY OF WOOD DALE AND THE ILLINOIS DEPARTMENT OF TRANSPORTATION (IDOT) PRIOR TO CONSTRUCTION. THE CONTRACTOR SHALL BE RESPONSIBLE FOR PROTECTING ALL UTILITIES AND STRUCTURES TO REMAIN. ALL UTILITIES SHALL BE DEEPENED TO A MINIMUM OF 48" BELOW FINISH GRADE. ALL UTILITIES SHALL BE COVERED WITH A MINIMUM OF 18" OF CONCRETE. ALL UTILITIES SHALL BE MARKED WITH RED X'S AND IDENTIFIED BY THE CONTRACTOR.

2. THE CONTRACTOR SHALL BE RESPONSIBLE FOR OBTAINING ALL NECESSARY PERMITS AND APPROVALS FROM THE CITY OF WOOD DALE AND THE ILLINOIS DEPARTMENT OF TRANSPORTATION (IDOT) PRIOR TO CONSTRUCTION. THE CONTRACTOR SHALL BE RESPONSIBLE FOR PROTECTING ALL UTILITIES AND STRUCTURES TO REMAIN. ALL UTILITIES SHALL BE DEEPENED TO A MINIMUM OF 48" BELOW FINISH GRADE. ALL UTILITIES SHALL BE COVERED WITH A MINIMUM OF 18" OF CONCRETE. ALL UTILITIES SHALL BE MARKED WITH RED X'S AND IDENTIFIED BY THE CONTRACTOR.

3. THE CONTRACTOR SHALL BE RESPONSIBLE FOR OBTAINING ALL NECESSARY PERMITS AND APPROVALS FROM THE CITY OF WOOD DALE AND THE ILLINOIS DEPARTMENT OF TRANSPORTATION (IDOT) PRIOR TO CONSTRUCTION. THE CONTRACTOR SHALL BE RESPONSIBLE FOR PROTECTING ALL UTILITIES AND STRUCTURES TO REMAIN. ALL UTILITIES SHALL BE DEEPENED TO A MINIMUM OF 48" BELOW FINISH GRADE. ALL UTILITIES SHALL BE COVERED WITH A MINIMUM OF 18" OF CONCRETE. ALL UTILITIES SHALL BE MARKED WITH RED X'S AND IDENTIFIED BY THE CONTRACTOR.

4. THE CONTRACTOR SHALL BE RESPONSIBLE FOR OBTAINING ALL NECESSARY PERMITS AND APPROVALS FROM THE CITY OF WOOD DALE AND THE ILLINOIS DEPARTMENT OF TRANSPORTATION (IDOT) PRIOR TO CONSTRUCTION. THE CONTRACTOR SHALL BE RESPONSIBLE FOR PROTECTING ALL UTILITIES AND STRUCTURES TO REMAIN. ALL UTILITIES SHALL BE DEEPENED TO A MINIMUM OF 48" BELOW FINISH GRADE. ALL UTILITIES SHALL BE COVERED WITH A MINIMUM OF 18" OF CONCRETE. ALL UTILITIES SHALL BE MARKED WITH RED X'S AND IDENTIFIED BY THE CONTRACTOR.

5. THE CONTRACTOR SHALL BE RESPONSIBLE FOR OBTAINING ALL NECESSARY PERMITS AND APPROVALS FROM THE CITY OF WOOD DALE AND THE ILLINOIS DEPARTMENT OF TRANSPORTATION (IDOT) PRIOR TO CONSTRUCTION. THE CONTRACTOR SHALL BE RESPONSIBLE FOR PROTECTING ALL UTILITIES AND STRUCTURES TO REMAIN. ALL UTILITIES SHALL BE DEEPENED TO A MINIMUM OF 48" BELOW FINISH GRADE. ALL UTILITIES SHALL BE COVERED WITH A MINIMUM OF 18" OF CONCRETE. ALL UTILITIES SHALL BE MARKED WITH RED X'S AND IDENTIFIED BY THE CONTRACTOR.

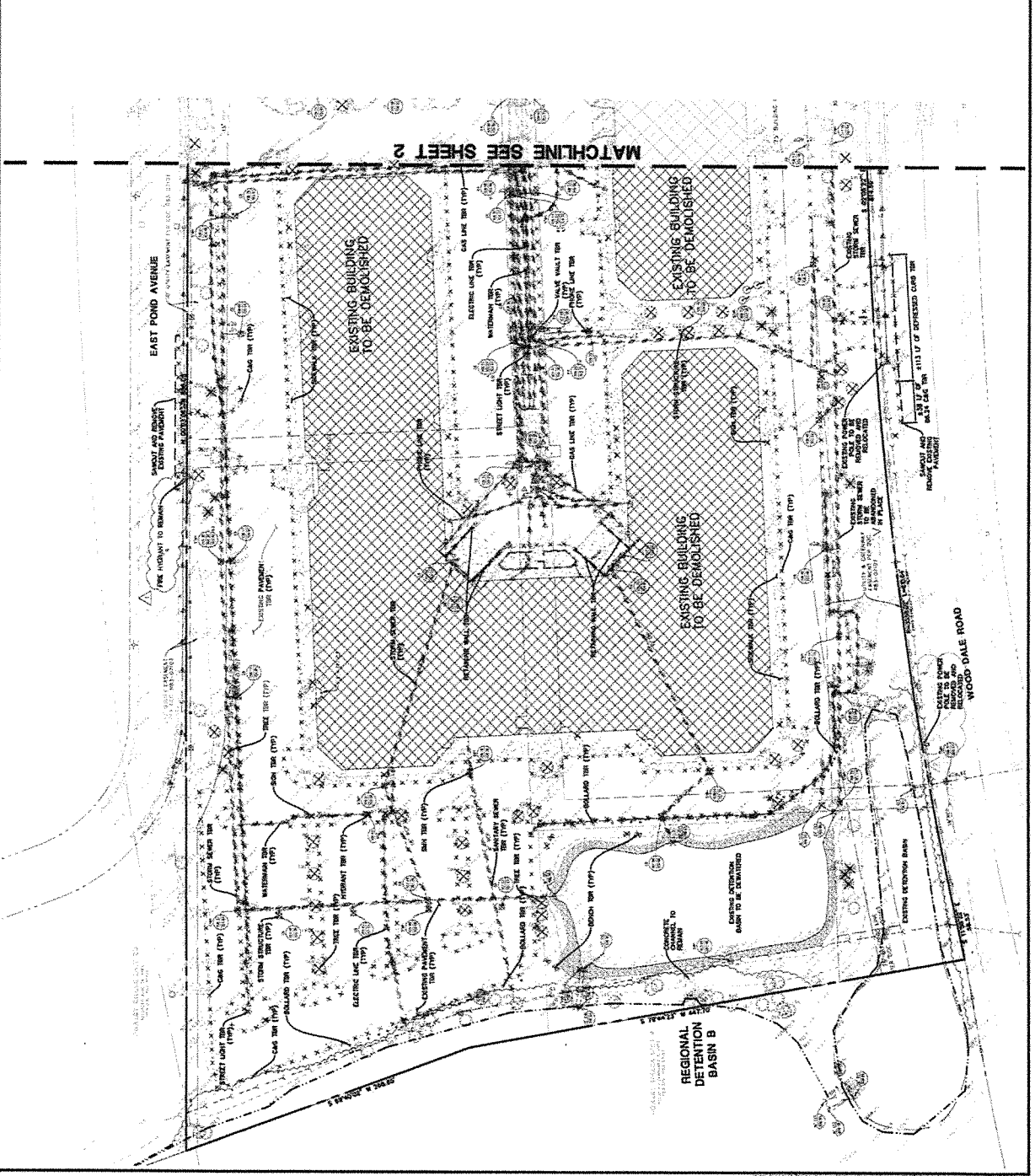
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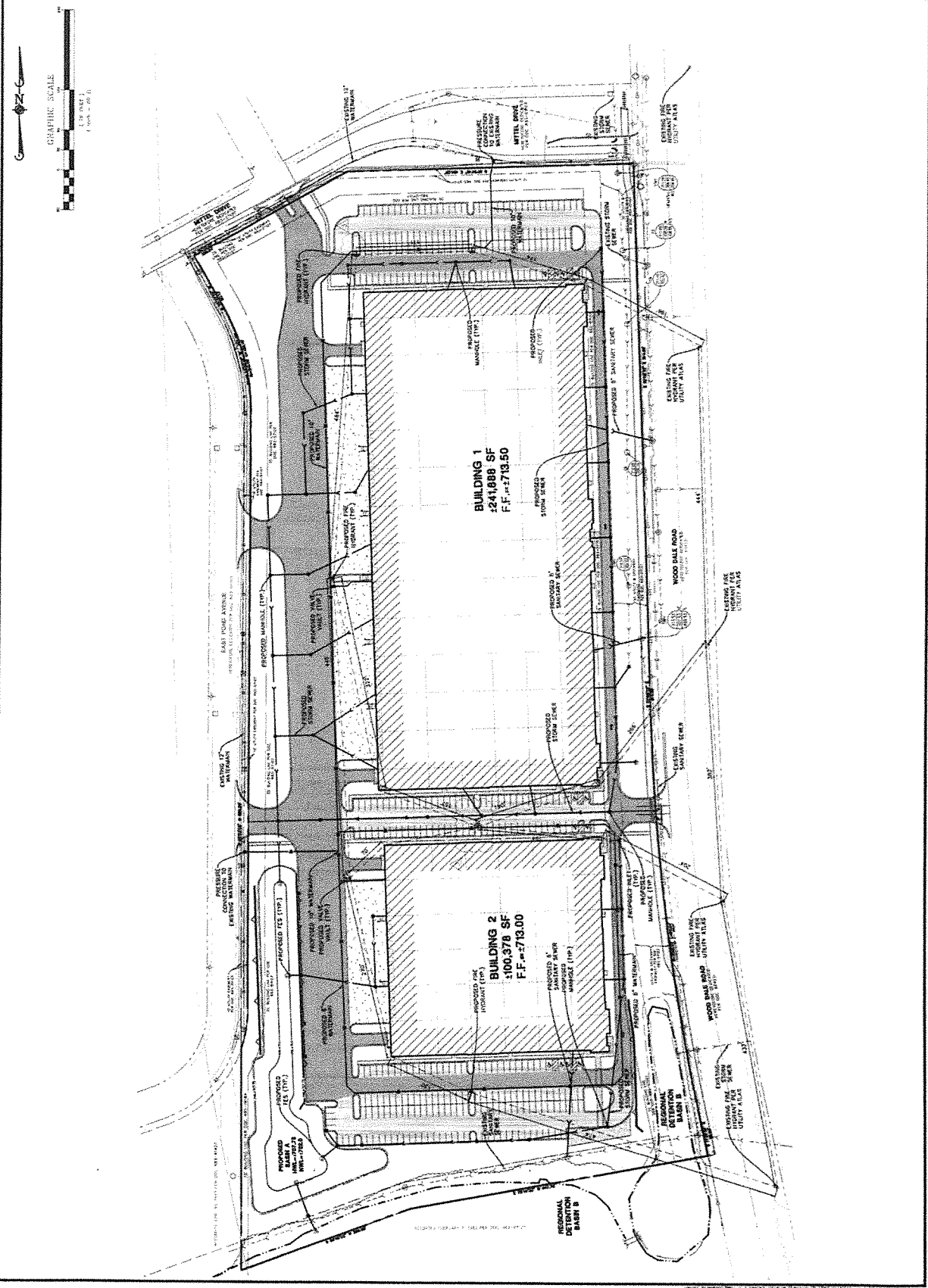
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OAKWOOD COMMERCE CENTER REDEVELOPMENT  
CITY OF WOOD DALE, ILLINOIS  
OVERALL UTILITY PLAN



GRAPHIC SCALE  
1" = 40'

PACKET 1 (REV. 03/27/24)



**PAVEMENT LEGEND**

- EXISTING MILL/ASPHALT, 1.5" MINIMUM THICKNESS (INCLUDES EXISTING MILL/ASPHALT, 1.5" MINIMUM THICKNESS, AND EXISTING MILL/ASPHALT, 1.5" MINIMUM THICKNESS)
- EXISTING CONCRETE, 4" COMPACTED AGGREGATE BASE, TYPE B
- PROPOSED MILL/ASPHALT, 1.5" MINIMUM THICKNESS (INCLUDES EXISTING MILL/ASPHALT, 1.5" MINIMUM THICKNESS, AND EXISTING MILL/ASPHALT, 1.5" MINIMUM THICKNESS)
- PROPOSED CONCRETE, 4" COMPACTED AGGREGATE BASE, TYPE B
- PROPOSED ASPHALT, 1.5" MINIMUM THICKNESS (INCLUDES EXISTING MILL/ASPHALT, 1.5" MINIMUM THICKNESS, AND EXISTING MILL/ASPHALT, 1.5" MINIMUM THICKNESS)
- PROPOSED CONCRETE, 4" COMPACTED AGGREGATE BASE, TYPE B

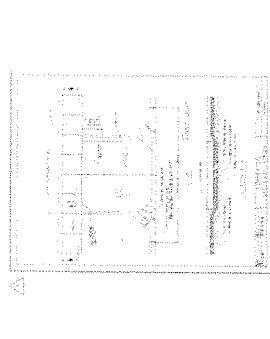
**GRAPHIC SCALE:**  
 1" = 10' (PLAN)  
 1" = 10' (ELEV.)

**SITE DATA**

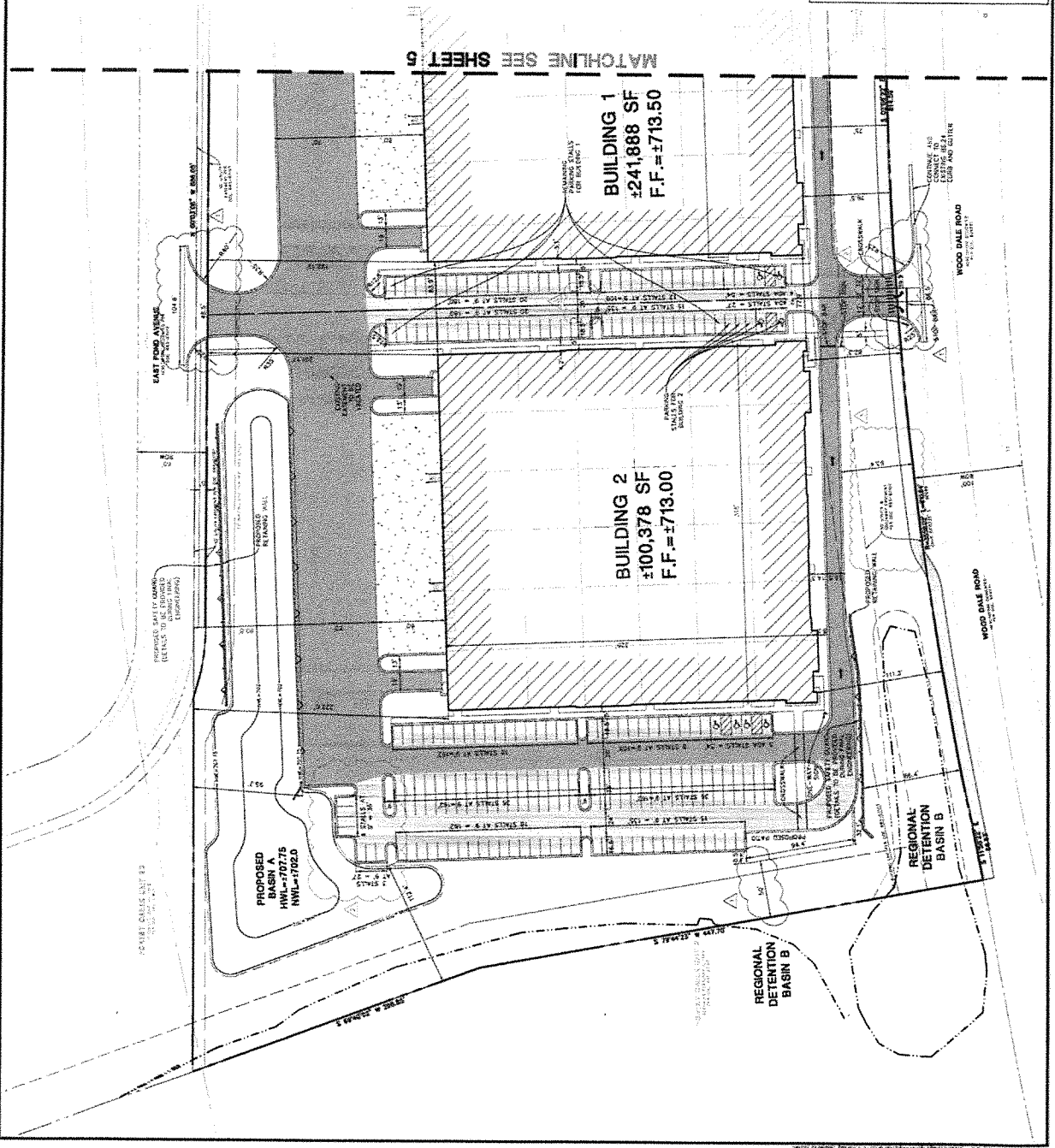
- LOT 1: 117,235 SF (0.112 AC)
- LOT 2: 117,235 SF (0.112 AC)
- LOT 3: 117,235 SF (0.112 AC)
- LOT 4: 117,235 SF (0.112 AC)
- LOT 5: 117,235 SF (0.112 AC)
- LOT 6: 117,235 SF (0.112 AC)
- LOT 7: 117,235 SF (0.112 AC)
- LOT 8: 117,235 SF (0.112 AC)
- LOT 9: 117,235 SF (0.112 AC)
- LOT 10: 117,235 SF (0.112 AC)
- LOT 11: 117,235 SF (0.112 AC)
- LOT 12: 117,235 SF (0.112 AC)
- LOT 13: 117,235 SF (0.112 AC)
- LOT 14: 117,235 SF (0.112 AC)
- LOT 15: 117,235 SF (0.112 AC)
- LOT 16: 117,235 SF (0.112 AC)
- LOT 17: 117,235 SF (0.112 AC)
- LOT 18: 117,235 SF (0.112 AC)
- LOT 19: 117,235 SF (0.112 AC)
- LOT 20: 117,235 SF (0.112 AC)
- LOT 21: 117,235 SF (0.112 AC)
- LOT 22: 117,235 SF (0.112 AC)
- LOT 23: 117,235 SF (0.112 AC)
- LOT 24: 117,235 SF (0.112 AC)
- LOT 25: 117,235 SF (0.112 AC)
- LOT 26: 117,235 SF (0.112 AC)
- LOT 27: 117,235 SF (0.112 AC)
- LOT 28: 117,235 SF (0.112 AC)
- LOT 29: 117,235 SF (0.112 AC)
- LOT 30: 117,235 SF (0.112 AC)
- LOT 31: 117,235 SF (0.112 AC)
- LOT 32: 117,235 SF (0.112 AC)
- LOT 33: 117,235 SF (0.112 AC)
- LOT 34: 117,235 SF (0.112 AC)
- LOT 35: 117,235 SF (0.112 AC)
- LOT 36: 117,235 SF (0.112 AC)
- LOT 37: 117,235 SF (0.112 AC)
- LOT 38: 117,235 SF (0.112 AC)
- LOT 39: 117,235 SF (0.112 AC)
- LOT 40: 117,235 SF (0.112 AC)

**PARKING DATA**

POSITION	NUMBER	40000000	15000000	10000000	5000000	2500000	1500000
LOT 1	108	27	6	108	27	6	108
LOT 2	108	27	6	108	27	6	108



- SITE DIMENSIONAL AND PAVING NOTES:**
- ALL DIMENSIONS ARE FACE TO FACE OF CURB OR EXISTING CONSTRUCTION.
  - ALL PROPOSED CURB AND GUTTER SHALL BE 18" UNLESS OTHERWISE NOTED.
  - ALL EXISTING CURB SHALL BE 18" UNLESS OTHERWISE NOTED.
  - 1" LONG SLOTTED METAL EXISTING CURB FACING CURB AND GUTTER WITH 4" DEPTH.
  - EXISTING DIMENSIONS AND EXISTING PAVING ARE TO BE MAINTAINED EXCEPT AS SHOWN OTHERWISE.
  - PROPOSED DIMENSIONS AND PAVING ARE TO BE MAINTAINED EXCEPT AS SHOWN OTHERWISE.
  - ALL DIMENSIONS ARE FACE TO FACE OF CURB OR EXISTING CONSTRUCTION.
  - ALL DIMENSIONS ARE FACE TO FACE OF CURB OR EXISTING CONSTRUCTION.
  - ALL DIMENSIONS ARE FACE TO FACE OF CURB OR EXISTING CONSTRUCTION.
  - ALL DIMENSIONS ARE FACE TO FACE OF CURB OR EXISTING CONSTRUCTION.
  - ALL DIMENSIONS ARE FACE TO FACE OF CURB OR EXISTING CONSTRUCTION.
  - ALL DIMENSIONS ARE FACE TO FACE OF CURB OR EXISTING CONSTRUCTION.
  - ALL DIMENSIONS ARE FACE TO FACE OF CURB OR EXISTING CONSTRUCTION.
  - ALL DIMENSIONS ARE FACE TO FACE OF CURB OR EXISTING CONSTRUCTION.
  - ALL DIMENSIONS ARE FACE TO FACE OF CURB OR EXISTING CONSTRUCTION.



**GRAPHIC SCALE**  
 1" = 20' (AS SHOWN)

**CONSTRUCTION NOTES**

PROPOSED FOOT CANTILEVER	PROPOSED FLOOR FINISH ELEVATION	PROPOSED GRASS	PROPOSED TRUNKS OF TREES
PROPOSED EXISTING FLOOR FINISH ELEVATION	PROPOSED SIDEWALK	PROPOSED DRIVEWAY	PROPOSED DRIVEWAY
PROPOSED EXISTING DRIVEWAY	PROPOSED EXISTING SIDEWALK	PROPOSED EXISTING SIDEWALK	PROPOSED EXISTING SIDEWALK
PROPOSED EXISTING SIDEWALK	PROPOSED EXISTING SIDEWALK	PROPOSED EXISTING SIDEWALK	PROPOSED EXISTING SIDEWALK
PROPOSED EXISTING SIDEWALK	PROPOSED EXISTING SIDEWALK	PROPOSED EXISTING SIDEWALK	PROPOSED EXISTING SIDEWALK
PROPOSED EXISTING SIDEWALK	PROPOSED EXISTING SIDEWALK	PROPOSED EXISTING SIDEWALK	PROPOSED EXISTING SIDEWALK
PROPOSED EXISTING SIDEWALK	PROPOSED EXISTING SIDEWALK	PROPOSED EXISTING SIDEWALK	PROPOSED EXISTING SIDEWALK



**BUILDING 1**  
 ±241,888 SF  
 F.F. = ±713.50

**MATCHLINE SEE SHEET 8**

**ING B8 S 713.5**

**7-10**

**PRELIMINARY ENGINEERING PLANS**

1. RETURN WALL DESIGN TO BE PROVIDED BY OWNER.

2. ALL FOUNDATION WALLS SHALL BE CONSTRUCTED WITH A MINIMUM CROSS SECTION OF 18" x 18" REINFORCED CONCRETE WITH A MINIMUM COVER OF 3" TO ALL REINFORCEMENT.

3. CONTRACTOR SHALL VERIFY ALL EXISTING FOUNDATION WALLS AND FOOTINGS ARE SOUND AND SUFFICIENT FOR THE PROPOSED STRUCTURE. ANY DEFICIENCIES SHALL BE CORRECTED BY THE CONTRACTOR AT HIS OWN RISK AND WITHOUT DELAY.

4. CONTRACTOR SHALL VERIFY ALL EXISTING FOUNDATION WALLS AND FOOTINGS ARE SOUND AND SUFFICIENT FOR THE PROPOSED STRUCTURE. ANY DEFICIENCIES SHALL BE CORRECTED BY THE CONTRACTOR AT HIS OWN RISK AND WITHOUT DELAY.

5. CONTRACTOR SHALL VERIFY ALL EXISTING FOUNDATION WALLS AND FOOTINGS ARE SOUND AND SUFFICIENT FOR THE PROPOSED STRUCTURE. ANY DEFICIENCIES SHALL BE CORRECTED BY THE CONTRACTOR AT HIS OWN RISK AND WITHOUT DELAY.

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11. CONTRACTOR SHALL VERIFY ALL EXISTING FOUNDATION WALLS AND FOOTINGS ARE SOUND AND SUFFICIENT FOR THE PROPOSED STRUCTURE. ANY DEFICIENCIES SHALL BE CORRECTED BY THE CONTRACTOR AT HIS OWN RISK AND WITHOUT DELAY.

**GENERAL NOTES:**

1. ALL FOUNDATION WALLS SHALL BE CONSTRUCTED WITH A MINIMUM CROSS SECTION OF 18" x 18" REINFORCED CONCRETE WITH A MINIMUM COVER OF 3" TO ALL REINFORCEMENT.

2. CONTRACTOR SHALL VERIFY ALL EXISTING FOUNDATION WALLS AND FOOTINGS ARE SOUND AND SUFFICIENT FOR THE PROPOSED STRUCTURE. ANY DEFICIENCIES SHALL BE CORRECTED BY THE CONTRACTOR AT HIS OWN RISK AND WITHOUT DELAY.

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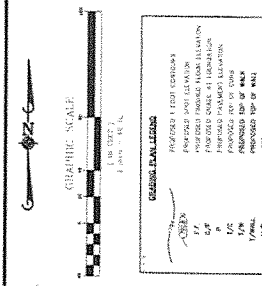
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**CONTRACTOR SHALL VERIFY ALL EXISTING FOUNDATION WALLS AND FOOTINGS ARE SOUND AND SUFFICIENT FOR THE PROPOSED STRUCTURE. ANY DEFICIENCIES SHALL BE CORRECTED BY THE CONTRACTOR AT HIS OWN RISK AND WITHOUT DELAY.**

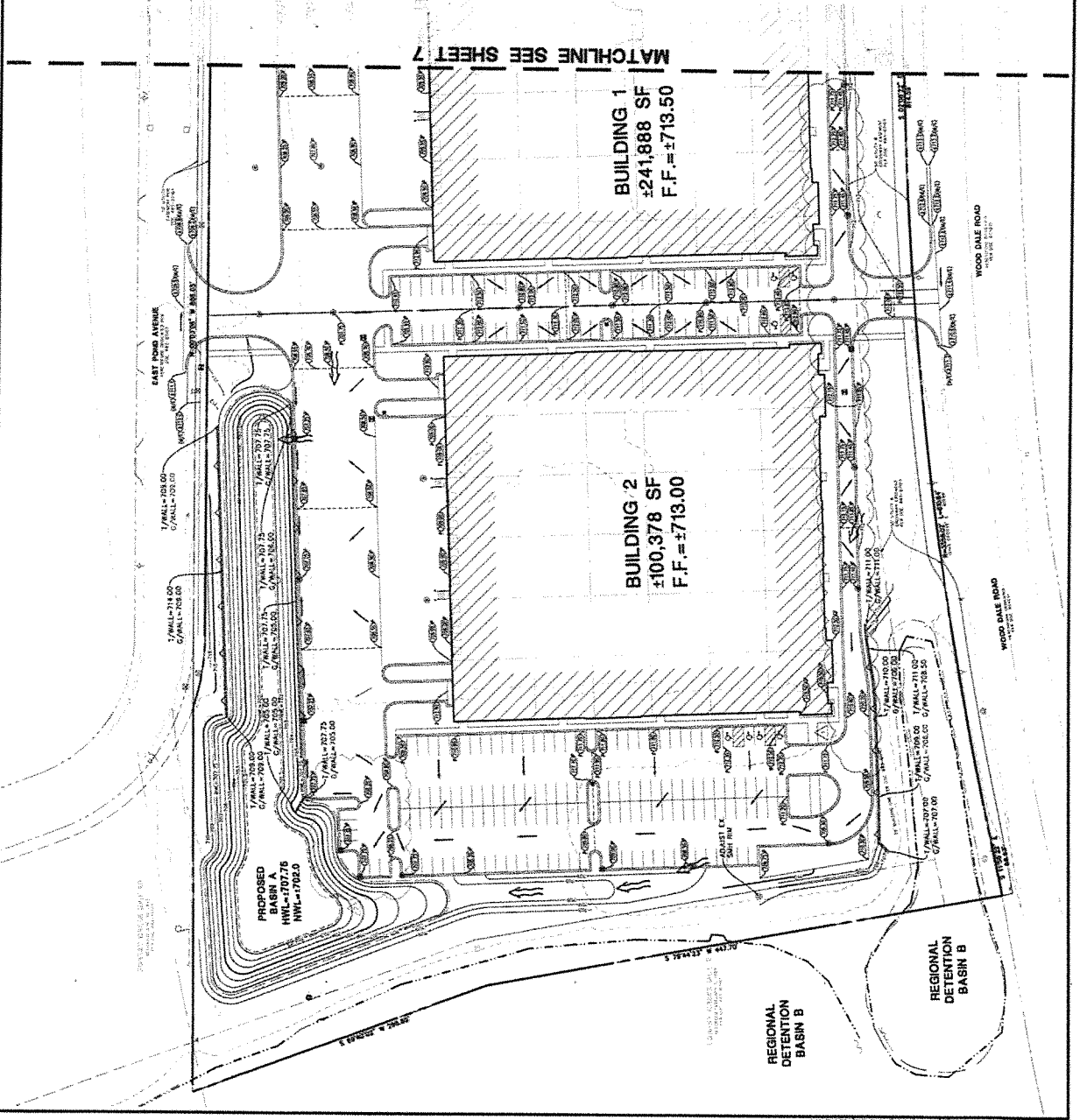




**LEGEND**

PROPOSED 1 FOOT GRADATIONS
PROPOSED 2 FOOT GRADATIONS
PROPOSED 3 FOOT GRADATIONS
PROPOSED 4 FOOT GRADATIONS
PROPOSED 5 FOOT GRADATIONS
PROPOSED 6 FOOT GRADATIONS
PROPOSED 7 FOOT GRADATIONS
PROPOSED 8 FOOT GRADATIONS
PROPOSED 9 FOOT GRADATIONS
PROPOSED 10 FOOT GRADATIONS
PROPOSED 12 FOOT GRADATIONS
PROPOSED 15 FOOT GRADATIONS
PROPOSED 20 FOOT GRADATIONS
PROPOSED 25 FOOT GRADATIONS
PROPOSED 30 FOOT GRADATIONS
PROPOSED 35 FOOT GRADATIONS
PROPOSED 40 FOOT GRADATIONS
PROPOSED 45 FOOT GRADATIONS
PROPOSED 50 FOOT GRADATIONS

- GRADING NOTES:**
1. RETURNING WALL OR CURB TO BE PROVIDED BY OTHERS.
  2. ALL SLOPES SHALL BE THROUGH FINISHED ACCESSORY DRIVEWAY PAVING AREA GRAD. BY 2:2.5% MINIMUM IN ALL DIRECTIONS UNLESS OTHERWISE NOTED.
  3. ALL HANDICAP RAMP SHALL BE CONSTRUCTED WITH A MAXIMUM GRADE MAX. OF 1:12 OR LESS.
  4. ALL SLOPES SHALL BE FINISHED TO A MINIMUM OF 1% UNLESS OTHERWISE NOTED.
  5. ALL GRADING SHALL BE FINISHED TO A MINIMUM OF 1% UNLESS OTHERWISE NOTED.
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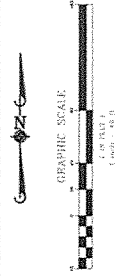


OAKWOOD COMMERCE CENTER REDEVELOPMENT  
CITY OF WOOD DALE, ILLINOIS  
UTILITY PLAN - NORTH

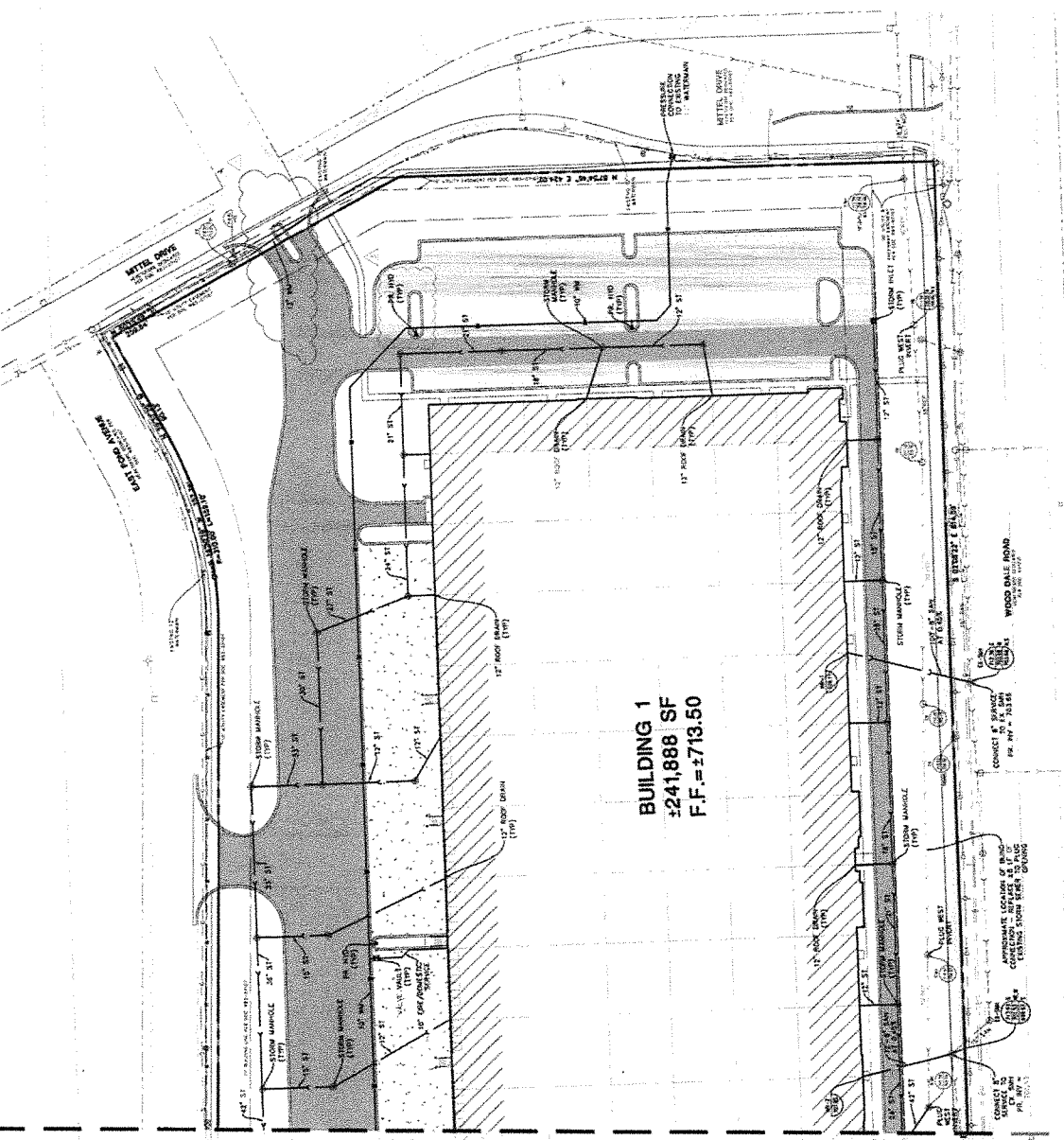
9' 10"



Table with project details including date, sheet number, and revision history.



- 1. ALL UTILITY ENGINEERS ARE TO SHOWN OF SIDE OR CENTER OF STRUCTURE UNLESS OTHERWISE NOTED.
- 2. ALL UTILITIES SHOWN ARE TO BE LOCATED AS SHOWN UNLESS OTHERWISE NOTED.
- 3. ALL UTILITIES SHOWN ARE TO BE LOCATED AS SHOWN UNLESS OTHERWISE NOTED.
- 4. ALL UTILITIES SHOWN ARE TO BE LOCATED AS SHOWN UNLESS OTHERWISE NOTED.
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- 10. ALL UTILITIES SHOWN ARE TO BE LOCATED AS SHOWN UNLESS OTHERWISE NOTED.
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- 15. ALL UTILITIES SHOWN ARE TO BE LOCATED AS SHOWN UNLESS OTHERWISE NOTED.
- 16. ALL UTILITIES SHOWN ARE TO BE LOCATED AS SHOWN UNLESS OTHERWISE NOTED.
- 17. ALL UTILITIES SHOWN ARE TO BE LOCATED AS SHOWN UNLESS OTHERWISE NOTED.



BUILDING 1  
±241,988 SF  
F.F. = ±713.50

MATCHLINE SEE SHEET 10  
ING 90 S 713.50





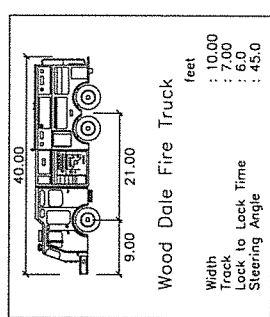
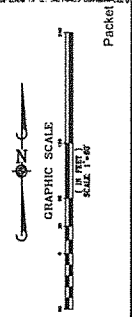
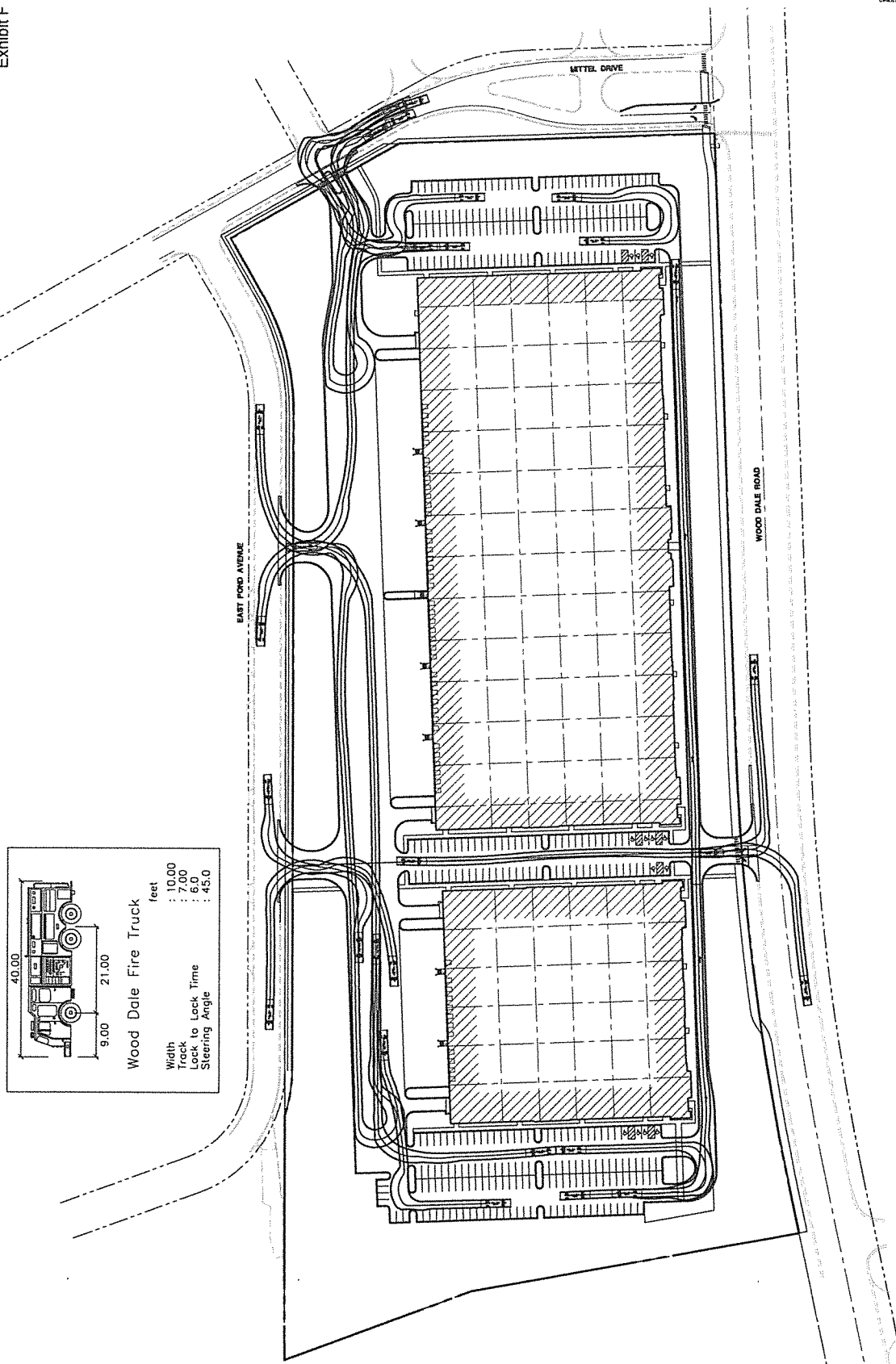
NO.	REVISIONS	DATE	BY



OAKWOOD COMMERCE CENTER REDEVELOPMENT  
CITY OF WOOD DALE, ILLINOIS  
AUTOTURN EXHIBIT - FIRE TRUCK

DATE	
BY	
CHECKED	
SCALE	AS SHOWN
SHEET	EX-1
PROJECT	OAKWOOD COMMERCE CENTER REDEVELOPMENT

Exhibit F

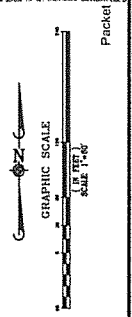
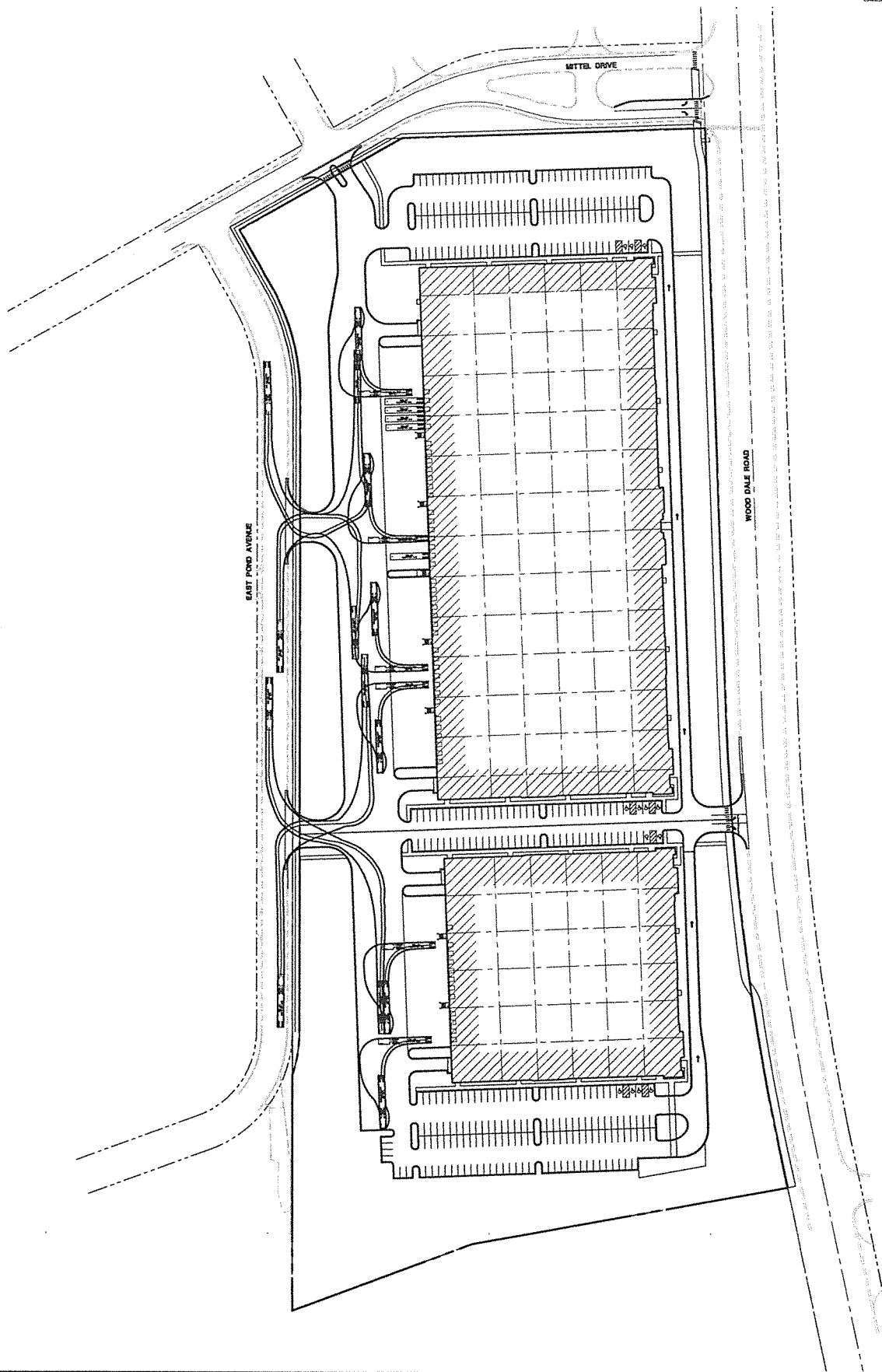


NO.	DATE	BY	REVISION

**Manhard**  
CONSULTING & ENGINEERING  
200 Progress Drive, Lombard, Illinois 60148  
Tel: 630.261.1234 Fax: 630.261.1235  
www.manhard.com

OAKWOOD COMMERCE CENTER REDEVELOPMENT  
CITY OF WOOD DALE, ILLINOIS  
AUTOTURN EXHIBIT - SEMI-TRUCK INBOUND

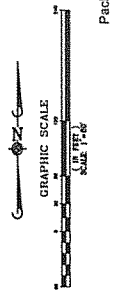
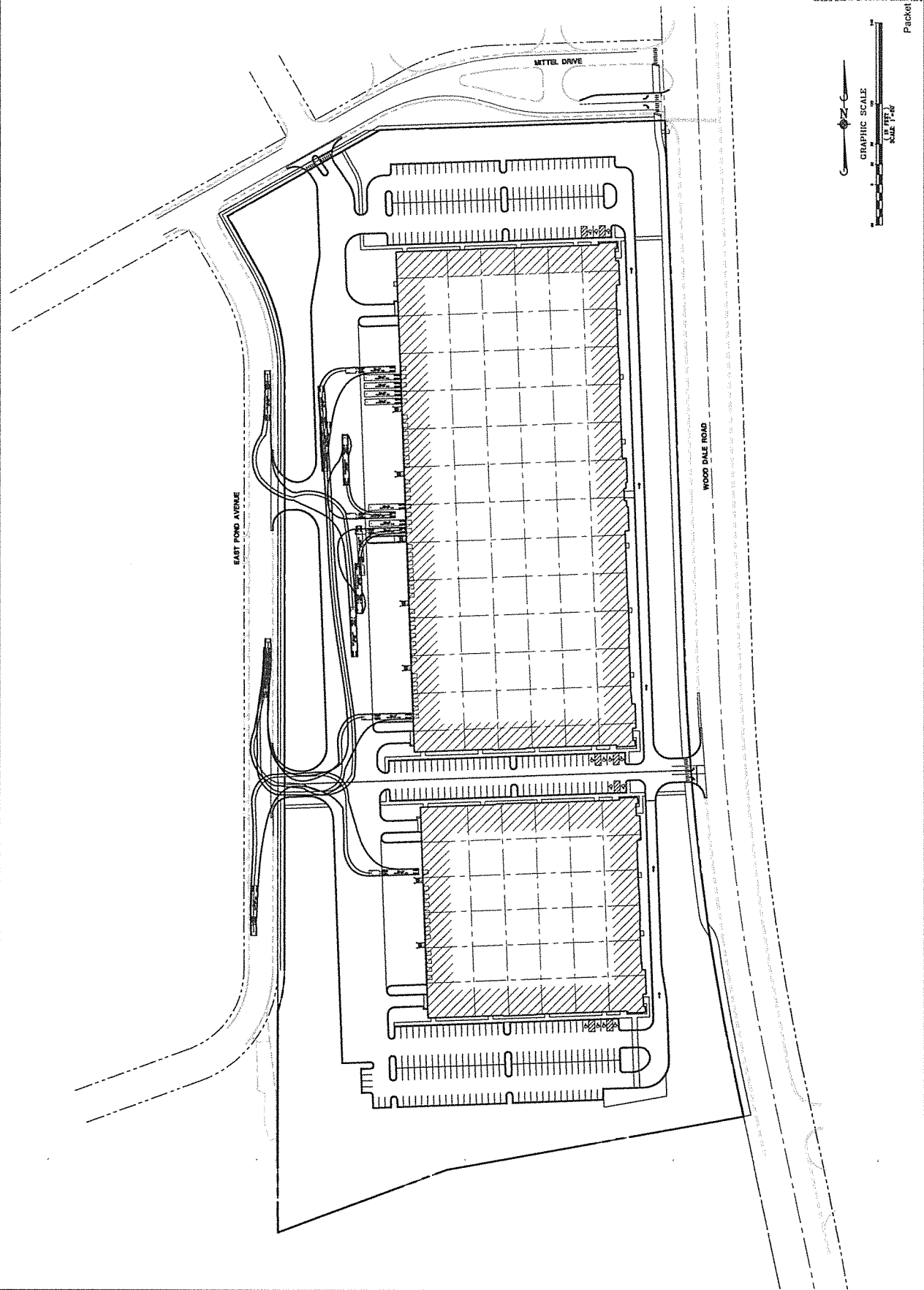
DATE: 08/11/11  
DRAWN: J. J. JONES  
CHECKED: J. J. JONES  
SCALE: AS SHOWN  
SHEET: EX2  
PROJECT: OAKWOOD COMMERCE CENTER REDEVELOPMENT  
PARCEL: 11000 S. WOOD DALE ROAD



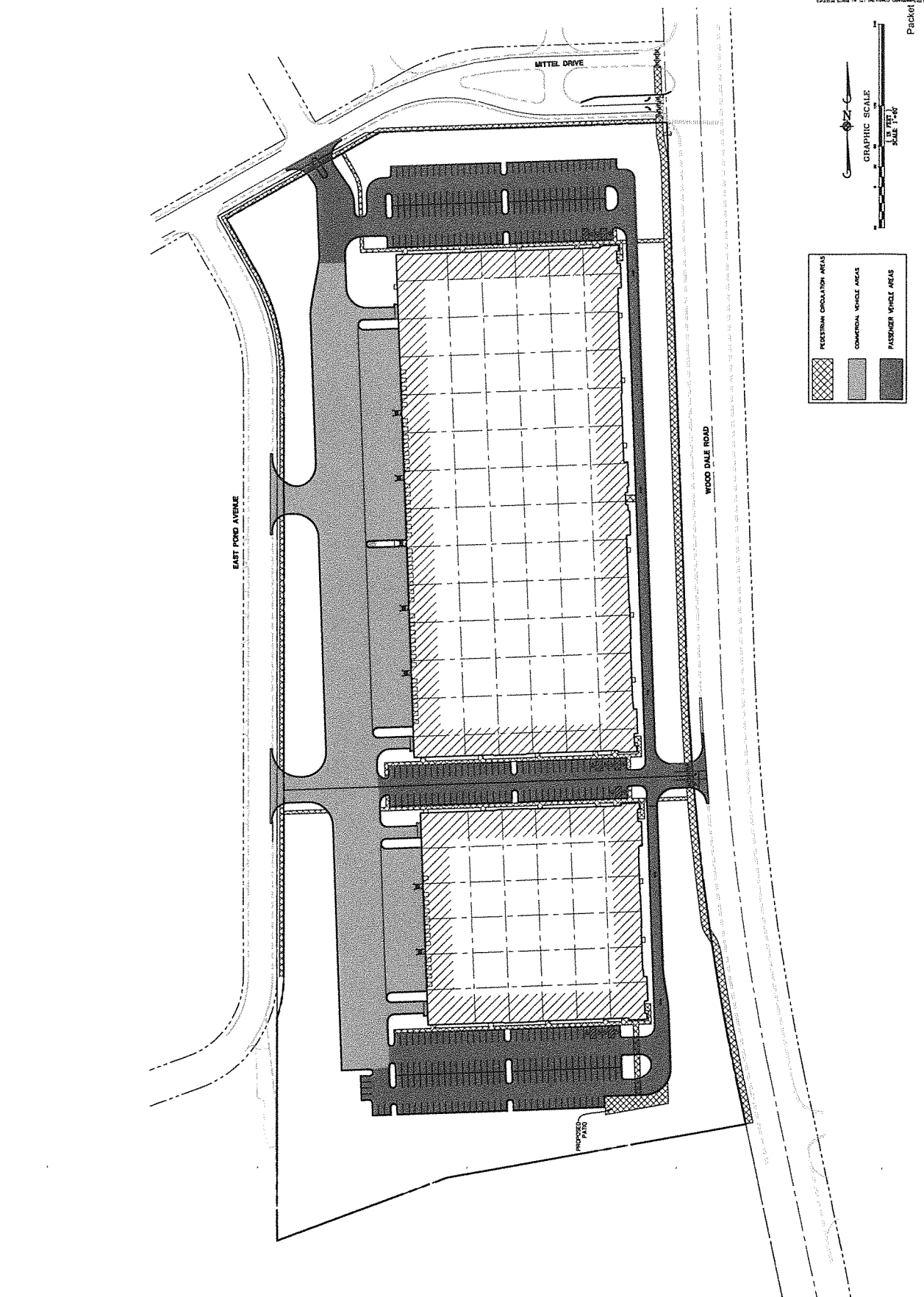
Parcel: 11000 S. WOOD DALE ROAD

DATE	12/15/2011
BY	J. J. JACOB
PROJECT	EXHIBIT
SCALE	AS SHOWN
NO.	1

NO.	DATE	DESCRIPTION



Packet 1 of 1



PACKET 11/11/14

The Landscape Plans are too large to include in this file but are available at:  
<http://www.wooddale.com/Home/ShowDocument?id=5219>

The Declaration of Easements, Covenants, Conditions and Restrictions are too large to include in this file but are available at:

<http://www.wooddale.com/Home/ShowDocument?id=5217>